



NEAR SOUTHSIDE Development Standards and Guidelines



Prepared by Near Southside, Inc.
in cooperation with the City of Fort Worth

February 2, 2016

(This page intentionally left blank)

TABLE OF CONTENTS**Section 1. Introduction**

- A. Background 3
- B. Promoting Revitalization with Form-Based Standards 3

Section 2. General Development Principles

- A. Intent 5
- B. General Development Principles 5

Section 3. Administration

- A. Intent 7
- B. Relationship to Other Regulatory Documents 7
- C. Definition of “Standards” and “Guidelines” 7
- D. Development Review Process 7-8
- E. Near Southside District Boundary 9

Section 4. Regulating Plan

- A. Intent and Principles 11
 - How to Use the Regulating Plan 11
 - The Transect Classification System 12
- B. Maps of Development Zones and Historic Properties 13-16
- C. Circulation Network and Thoroughfare Classification 17
 - Maps 18-21

Section 5. Development Standards and Guidelines

- A. Intent and Principles 23-24
- B. Streets and Other Public Spaces 25-34
 - Streets 25-30
 - Roadsides 31-33
 - Public Parks and Plazas 34
 - Public Art 34
- C. Building Location and Orientation 35-36
- D. Building Height 37
- E. Parking and Driveways 38
- F. Architectural Standards 39-40
- G. On-Site Landscaping 41
 - Canopy Tree List 42

Section 6. Permitted Land Uses

- A. Mix of Use Requirement for Large Developments 43
- B. Special Requirements for “N” and “R” Zones 44
- C. Permitted Land Uses 44-52

Section 7. Appendix

- A. Sign Guidelines (excerpted from Downtown Sign Standards)
- B. Zoning Ordinance Page for Near Southside “NS” District

(This page intentionally left blank)

1.A. Background

The Near Southside is located just south of Downtown Fort Worth and is the city's second largest employment center, with over 30,000 workers. Approximately 18,000 of these workers are employed in the health care industry. Although commonly recognized today as the "Medical District," the area has historically been a mixed-use district with diverse neighborhoods and eclectic destinations. The area's remaining historic buildings – with their architectural diversity, human-scale design, and wide range of locally-owned businesses – reflect this storied past.

Fort Worth South, Inc. (FWSI), a non-profit redevelopment corporation, has worked since 1996 with the City of Fort Worth and other community groups to promote the area's revitalization in a way that builds on this history and capitalizes on the district's eclectic, mixed-use, human-scale character to create a competitive advantage in attracting new residents and businesses. Urban design (i.e., the design of buildings, streets, and other public spaces, and the interrelationships among those elements) will continue to be a major factor in determining the district's economic, social, and cultural sustainability. Promoting the sustainability of neighborhoods and commercial districts is a primary goal for the City of Fort Worth.

1.B. Promoting Revitalization with Form-Based Development Standards

As reflected in the City's Comprehensive Plan (which includes the FWSI strategic plan by reference), FWSI and the City have long recognized the importance of urban design in realizing revitalization goals for the Near Southside. As in the past, the district's future character will largely be determined by the individual design decisions made by hundreds of property owners, businesses, and residents over the following decades. The City's zoning regulations and other development standards play perhaps the most influential role in guiding these decisions, and, as a result, in shaping the Near Southside's character, quality of life, and sustainability.

For decades, zoning regulations, street design requirements, and other development standards have worked against urban design and redevelopment goals for the Near Southside and other central city districts. In response, the City adopted new form-based "MU" mixed-use zoning classifications, and, in certain central city areas such as Downtown and Trinity Uptown, created new urban design districts with tailored development standards and guidelines that promote development that is consistent with the contextual design goals for those respective areas.

FWSI has worked with property owners to promote desirable redevelopment in two ways: first, through rezonings to the "MU" classifications; and second, through the application of informal design guidelines. Although the effects of these past efforts have been significant, a comprehensive update of the district's development standards – through the creation of a new Near Southside urban design district overseen by the Urban Design Commission – is necessary to promote the district's continued revitalization, for several reasons:

- The patchwork of single-use industrial, commercial, and residential zoning districts did not allow or promote a **mix of residential, retail, office, and institutional uses** as envisioned in the City's Comprehensive Plan, within which the Near Southside is designated as a "mixed-use growth center."
- Outdated development standards did not promote the type of **pedestrian-oriented urban design** that characterizes the Near Southside's most distinctive and memorable areas and serves as a competitive advantage for the district.
- The patchwork of zoning districts threatened redevelopment momentum by failing to create a **predictable investment environment** in which property owners could confidently invest in redevelopment projects that conformed to the district's vision and be reasonably assured that zoning standards would require compatible development on adjacent properties.
- The wide range of conditions within the district – from lower density residential neighborhoods to large institutional campuses – required a **more tailored approach than simply applying all of the "MU" standards** throughout the area.
- Design excellence, administrative flexibility, and timely approval of conforming projects would best be promoted through a **development review process overseen by a well-qualified design review board**, as demonstrated by the Downtown Urban Design District process.

This illustrated document uses photographs and other images in an effort to create a user-friendly manual for use by professional developers as well as non-experts. The standards and guidelines contained are largely based on the form-based standards of the "MU" classifications. Form-based standards differ from conventional zoning standards in their emphasis on shaping the form of buildings and public spaces rather than the segregation of different land uses.

(This page intentionally left blank)

2.A. Intent

The Near Southside will become a vibrant, mixed-use, urban district if certain general development principles shape its redevelopment. The following principles serve as the basis for the standards and guidelines contained in this document. Development projects are evaluated with respect to the principles, in addition to the pertinent standards and guidelines.

2.B. General Development Principles

1. **Promote a pedestrian-oriented urban form.**

In contrast to conventional zoning standards that place primary emphasis on the regulation of land uses, the Near Southside development standards and guidelines focus on promoting a walkable, urban form of development, consistent with the district's historic urban character. The focus on form promotes buildings that conform to tested urban design principles, and that adapt to changing conditions over time.

2. **Maximize connectivity and access.**

The Near Southside's successful revitalization requires a truly multimodal circulation network in which residents, workers, and visitors may conveniently walk, drive, bike, or ride public transportation to destinations within and outside of the district. Development standards and guidelines are intended to promote walkable blocks and street designs that balance these transportation modes, and also ensure accessibility for all residents and visitors, including those with disabilities.

3. **Require excellence in the design of the public realm (building on Fort Worth's history of civic art) and of buildings that front public spaces.**

The most successful and memorable urban environments are those in which walking down the street is appealing. Streets, plazas, parks, and other public spaces should be comfortable and inviting, and buildings fronting those spaces should be active and visually interesting at the pedestrian level.

4. **Promote the preservation and creation of distinctive neighborhoods that provide diverse urban housing options.**

The vision for the Near Southside places high priority on the creation of mixed-use, mixed-income neighborhoods that include townhouses, apartments, condominiums, and supporting neighborhood-scale businesses. Providing a large number of units and options for various household types and income levels is critical to the district's success.

5. **Support existing Near Southside businesses.**

The Near Southside Development Standards and Guidelines should support the success of existing businesses.

6. **Encourage adaptive reuse and support the preservation of historically significant buildings.**

The Near Southside includes historically significant buildings that should be incorporated into new development projects. The development standards and guidelines are intended to work in conjunction with the City's Historic Preservation Ordinance to encourage preservation and adaptive reuse of significant buildings.

7. **Encourage creativity, architectural diversity, and exceptional design.**

The Near Southside Development Standards and Guidelines promote high quality design, and the development review process promotes flexibility. Standards and guidelines, as well as the development review process, are intended to support creativity and exceptional design while discouraging uniformity.

8. **Promote sustainable development that minimizes negative impacts on natural resources.**

Creating a walkable, mixed-use, high-density, central city district supports sustainable development by providing an alternative to low-density development in peripheral areas. In accordance with sustainable development principles, the Near Southside's buildings, transportation systems, and public spaces should be designed to minimize negative impacts on air and water quality and promote innovation in environmental design.

9. **Encourage the integration of public art into public and private development.**

Public art should be integrated into architecture, streetscapes, and public spaces. Public art will enhance the built environment and contribute to the area's success in attracting new residents and businesses.

(This page intentionally left blank)

3.A. Intent

The development standards and guidelines included in this document are intended to be clear, concise, and user-friendly. The development review process is intended to be predictable and flexible, and to facilitate the timely approval of conforming projects.

3.B. Relationship to Other Regulatory Documents

The Near Southside (NS) Development Standards and Guidelines document is intended to minimize references to other sections of the City's Zoning and Subdivision Ordinances. Developers are responsible, however, for reviewing and complying with all pertinent zoning and subdivision standards, including those not directly referenced in this document. There are three notable references to other sections of the Zoning Ordinance: a) the Historic Preservation Ordinance (Chapter 4, Article 5), which includes the procedures and requirements related to the preservation and reuse of historically significant buildings; b) Off-Street Parking and Loading (Chapter 6, Article 2), and c) the Sign Ordinance (Chapter 6, Article 4) and Downtown Sign Standards. The sign ordinance includes basic dimensional standards, and the Downtown sign standards are applied as design guidelines within the Near Southside district (see Appendix A). All development must comply with all federal, state, county, or city regulations. Historically designated properties must comply with requirements associated with those designations (e.g., Fairmount Historic District properties must comply with that district's design guidelines as well as processes required by the Historic and Cultural Landmarks Commission).

3.C. Definition of "Standards" and "Guidelines"

Development Standards are objective, measurable regulations, often illustrated through diagrams and sketches, with which all projects must comply. Unless noted as a guideline, all provisions in this document are development standards. If a project of exceptional design is clearly consistent with the General Development Principles but does not conform to a certain development standard, the Urban Design Commission (UDC) may approve a Certificate of Appropriateness (COA) that cites the project's compliance with those principles. (For projects not conforming to the height and setback regulations listed in the Zoning Ordinance as shown in Section 7, Appendix B, a variance approved by the Board of Adjustment is also required; see Development Review section below.)

Development Guidelines are more subjective statements through which the City proposes additional design strategies. The guidelines should be suitable for most projects, and developers should endeavor to ensure that guidelines are followed to the extent possible. City staff and the UDC will work with developers to explore design approaches that maximize conformance with guidelines. The UDC shall not deny a Certificate of Appropriateness solely because a project fails to comply with a guideline.

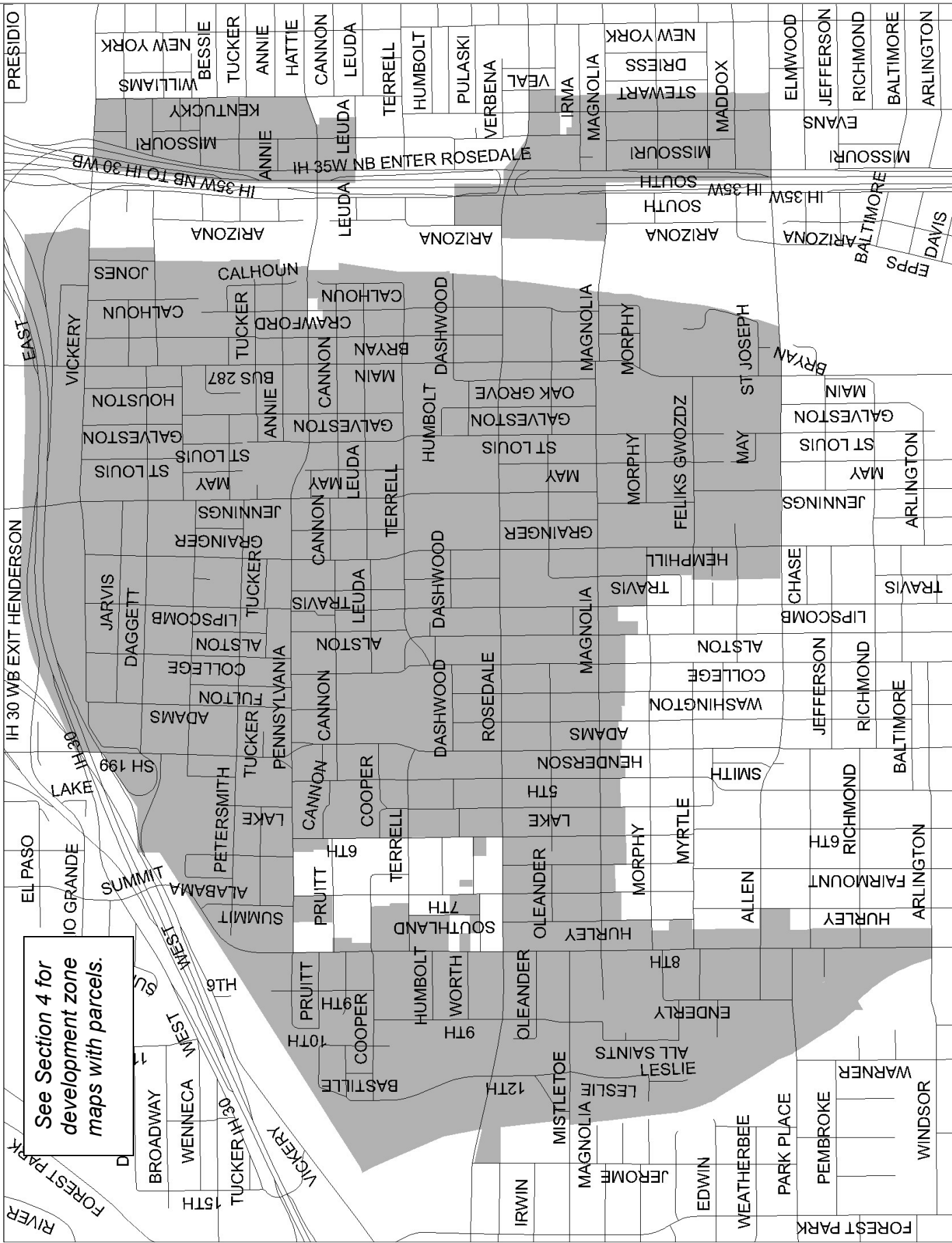
3.D. Development Review Process

1. **PROJECTS SUBJECT TO REVIEW** – All private construction projects, with the exception of interior construction or exterior in-kind replacement work, are subject to review by the Planning and Development Department and/or the Urban Design Commission (UDC) for compliance with the NS Standards and Guidelines. All buildings, streets, and public spaces by public entities are also subject to review.
2. **ADMINISTRATIVE REVIEW VS. UDC REVIEW** – Projects that are clearly consistent with all applicable development principles and that clearly conform to all standards may be approved administratively by the Planning and Development Director without UDC review. Because of the unique signage needs of the major hospitals, staff may also approve appropriate hospital signage that deviates from the dimensional standards. Additionally, staff may approve waivers from roadside design elements for properties located along a street that is planned for City or Fort Worth South Inc. sponsored streetscape projects. The UDC shall review projects that require interpretation or discretionary judgment with respect to the project's compliance with standards and guidelines. The UDC shall also review all of the following:
 - Skybridges (see Section 5.C.6.)
 - Drive-through Facilities (see Section 5.D.5.)
 - Mobile Food Courts (see Section 5.F.8.)
 - Monument Signs (see Section 5.F.9.a)

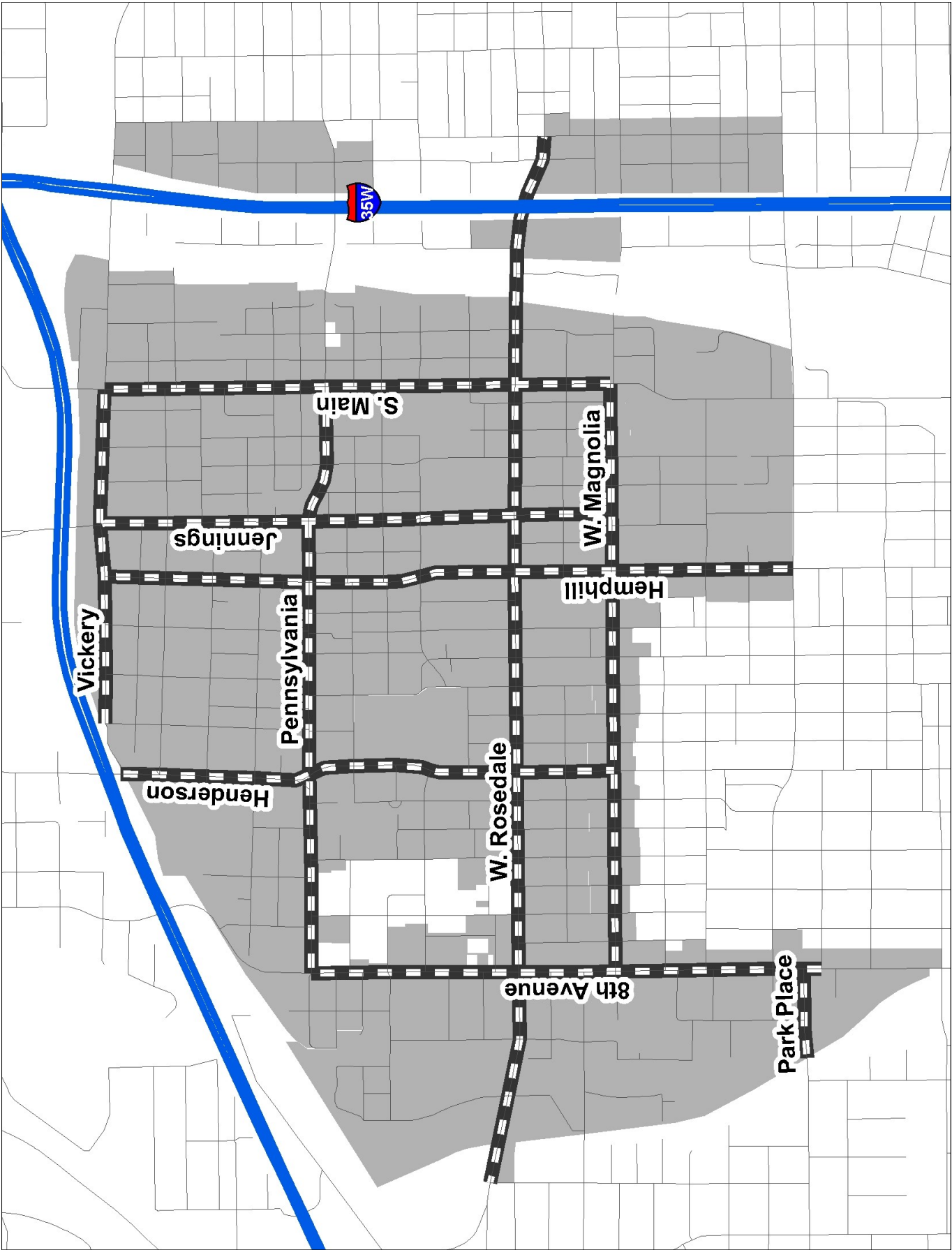
3.D. Development Review Process (cont.)

3. **URBAN DESIGN COMMISSION (UDC)** – The UDC is a nine-member body appointed by the City Council and charged with the administration of urban design districts. As stated above, the UDC shall review projects that require interpretation or discretionary judgment with respect to the project's compliance with standards and guidelines. The Zoning Ordinance specifies that UDC appointees should be knowledgeable about fundamental principles of urban design. Furthermore, at least six of the nine appointees shall be practicing professionals from the fields of architecture, landscape architecture, urban design or planning, real estate, or law. The Planning and Development Director serves as staff advisor to the UDC.
4. **CERTIFICATES OF APPROPRIATENESS** – The Planning and Development Director or UDC will issue a Certificate of Appropriateness (COA) for approved public and private projects. In exceptional cases the UDC may approve a COA if a project is clearly consistent with applicable development principles but does not conform to a certain development standard(s) or guideline(s). The COA for such projects must describe the projects' compliance with applicable principles.
5. **REQUIRED INFORMATION** – The full list of required materials is included in the Urban Design Commission application available from the City's Planning and Development Department. In general, the following information is required, as applicable:
 - a. Site Plan
 - b. Building Plans and Elevations
 - c. Landscape Plan
 - d. Material Specifications
 - e. Plans and Specifications for Proposed Signs
 - f. Description of Proposed Scope of Work
 - g. Photographs of Site and Existing Conditions
6. **REVIEW OF PHASED PROJECTS** – To minimize review steps for phased development projects, the UDC may approve a COA for all phases provided that the applicant submits drawings that depict the initial phase as well as all future phases. The COA is valid for two years from the date of approval.
7. **CONCEPTUAL PHASE DISCUSSIONS** – One of the UDC's most important roles is to provide informal guidance to developers of significant projects during the conceptual design phase. These discussions take place during monthly UDC work sessions, and provide an opportunity for early UDC and staff input that should facilitate timely design review and approval of the final design phase. City staff encourages all public and private developers of significant projects to present their conceptual plans during a UDC work session.
8. **VARIANCES TO ZONING AND SUBDIVISION ORDINANCES' PROPERTY DEVELOPMENT STANDARDS** – The UDC is authorized to approve appropriate exceptions to the vast majority of the NS Standards and Guidelines, provided a project complies with applicable development principles. The UDC may allow additional flexibility for projects of exceptional civic or environmental design. Any waiver of basic property development standards related to building heights or setbacks, however, requires a variance approved by the Board of Adjustment. Similarly, vacations of public rights-of-way and any waivers of subdivision requirements require approval by the City Plan Commission. Variances, vacations, and subdivision waivers should be recommended for approval by the UDC. The UDC is authorized to approve exceptions to roadside design standards.

3.E. Near Southside District Boundary



3.F. Near Southside Primary Streets



(This page intentionally left blank)

4.A. Intent and Principles

The Near Southside Regulating Plan functions similarly to both the Zoning Ordinance map and the Master Thoroughfare Plan map. The NS Regulating Plan includes maps that depict the boundaries of different types of development zones, as well as maps identifying the location and classification of thoroughfares. These maps serve as a guide for determining which development standards and guidelines apply to a development project. A unified section that classifies both development zones and streets is intended to promote coordination among land use, urban design, and transportation decisions, which is an essential strategy in creating a vibrant, livable, mixed-use district. The following principles guide the content of the Regulating Plan.

DEVELOPMENT ZONES

1. Draw development zone boundaries so as to enhance the character of existing neighborhoods and commercial districts while also promoting compatible higher-density, mixed-use redevelopment in appropriate locations.
2. Draw development zone boundaries to promote a large number of new residential units in order to create a “live, work, play” environment and to support neighborhood retail and other commercial uses.
3. Draw development zone boundaries to support large institutions and businesses that warrant tailored development standards based on their unique operational requirements.

HISTORIC PROPERTIES

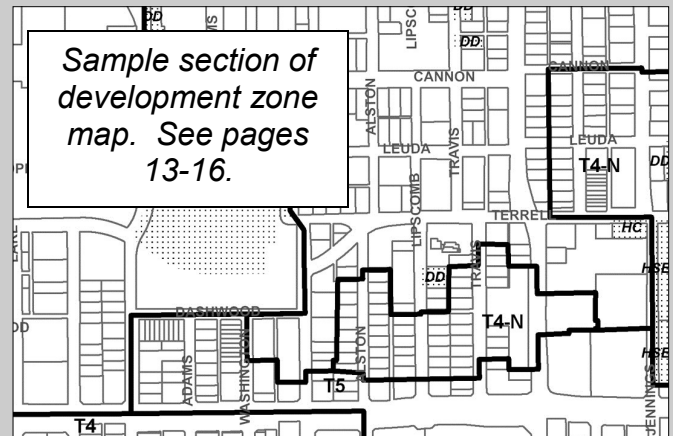
4. Facilitate the incorporation of historically significant buildings into redevelopment projects by identifying those properties on the Regulating Plan and by referencing the Zoning Ordinance’s Historic Preservation Ordinance and applicable incentives.
5. Facilitate the designation of currently unprotected historic properties and their adaptive reuse by identifying the most eligible properties on the Regulating Plan as information on those properties becomes available.

CIRCULATION NETWORK AND STREET CLASSIFICATION

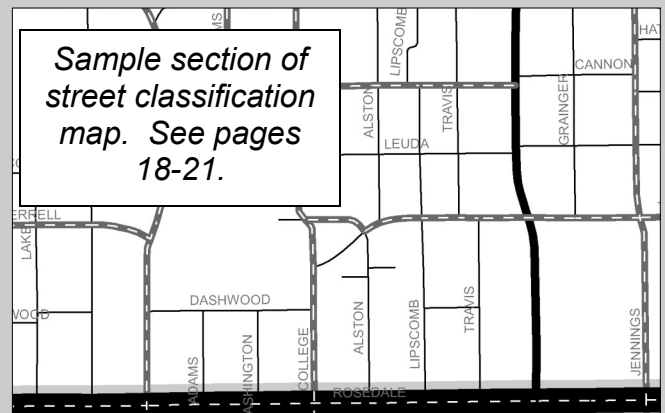
6. Preserve the existing urban street grid to maximize street connectivity for vehicles, pedestrians, public transportation, and bicycles. The grid promotes efficient circulation and provides a wide range of mobility options.
7. Limit vacations of existing public rights-of-way, including streets and alleys, only to situations where there is no adverse impact to future circulation and desirable redevelopment, or in cases where such vacations are absolutely necessary for an exceptional redevelopment project that is clearly consistent with the general development principles.
8. Utilize a context-sensitive street classification system that gives equal consideration to redevelopment and mobility goals.

HOW TO USE THE REGULATING PLAN

1. Locate the subject property on the applicable development zone and historic properties map in Section 4.B.



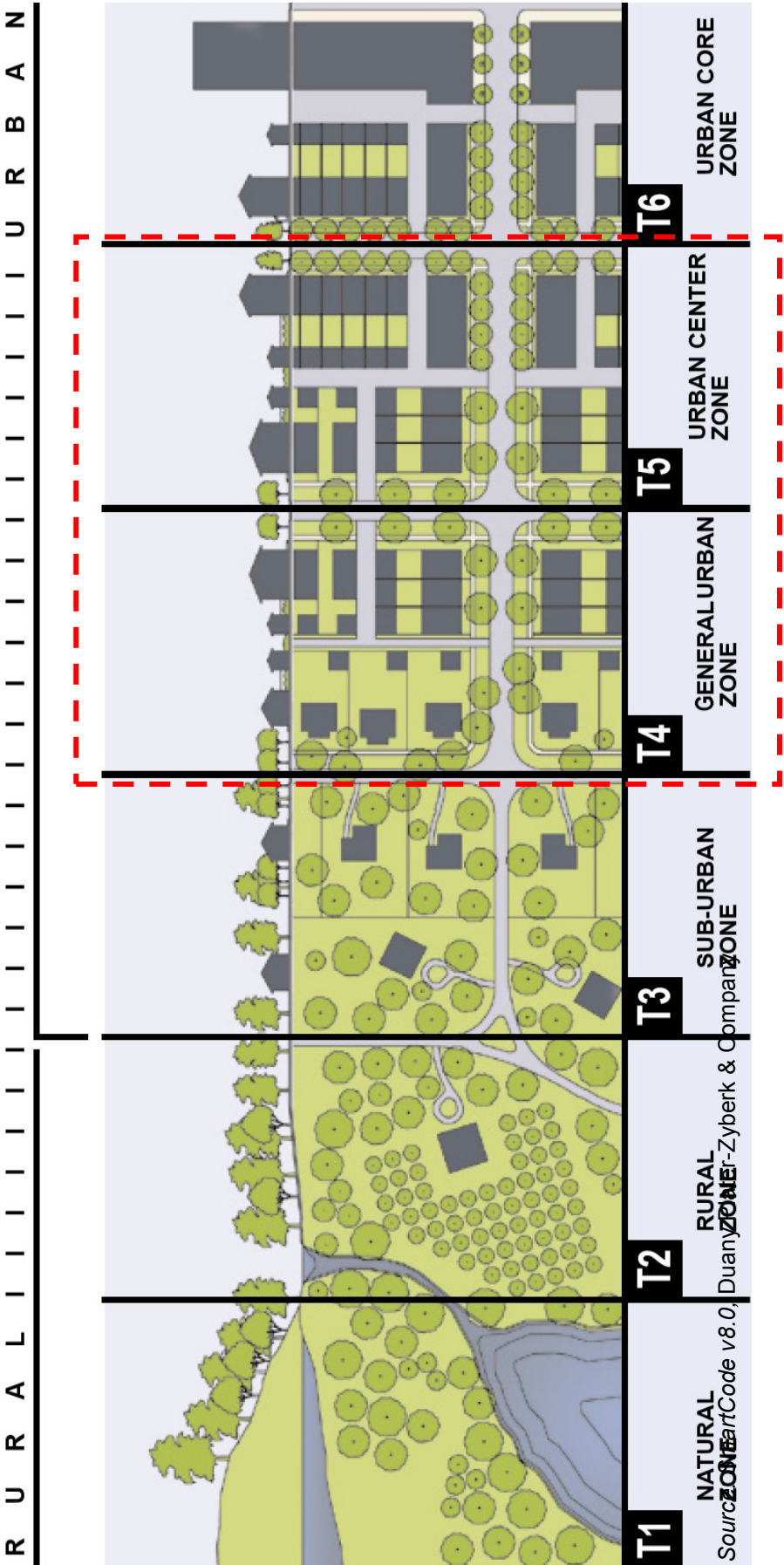
2. Identify the property’s development zone and any applicable historic designations.
3. Locate the subject property on the applicable circulation network and street classification map in Section 4.C.



4. Identify the classifications of all adjacent streets.
5. Refer to Sections 5-7 for applicable standards and guidelines related to streets, streetscapes, public spaces, sites, and buildings.
6. Refer to Chapter 4, Article 5 of the Zoning Ordinance for regulations pertaining to historic properties.
7. Refer to all other referenced sections of the Zoning Ordinance or other regulatory manuals.

The Transect Classification System

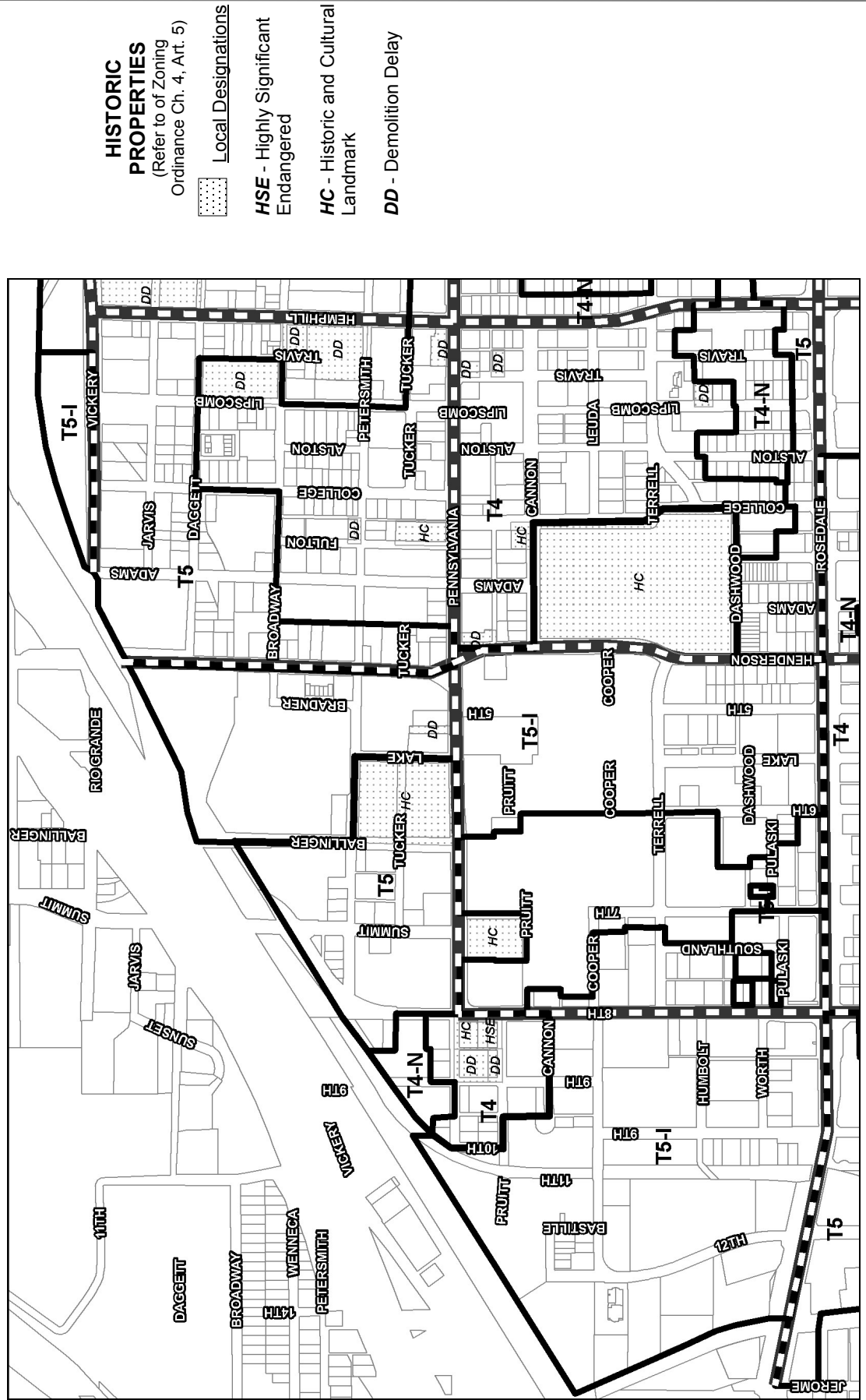
The diagram below illustrates the "Transect" that serves as the basis for many form-based development codes. The Transect diagram shows a continuous cross-section of human habitats, from rural to urban. Transect zones "T4: General Urban" and "T5: Urban Center" are most consistent with the redevelopment vision for the Near Southside. (The development standards for T4 and T5 are very similar to those for the MU-1 and MU-2 zoning districts, respectively.) T4 and T5 zones, as well as special neighborhood and institutional/industrial zones are shown on the development zone maps in the following section.



4.B. Development Zones and Historic Properties

T4	20-ft. maximum front setback 18-foot minimum façade height* 3 stories max. if single use 5 stories max. w/ public space or mix of uses 6 stories max. w/ public space and mix of uses Parking behind or beside buildings *unless located on "primary street"– see map page 9	T5	20-foot maximum front setback 18-foot minimum façade height* 5 stories max. if single use 8 stories w/ public space or mix of uses 10 stories w/ public space and mix of uses Parking behind or beside buildings	-N	Neighborhood zone No single-use non-residential	-R	Restricted zone No bars or industrial uses; see p. 44.
				-I	Institutional/Industrial zone Flexible driveway and entrance standards		

NORTHWEST QUADRANT



4.B. Development Zones and Historic Properties

2. SOUTHWEST

Basic Development Standards (Standards and guidelines are listed in Sections 5-7.)

T4

20-ft. maximum front setback
18-foot minimum façade height*
3 stories max. if single use
5 stories max. w/ public space or mix of uses
6 stories max. w/ public space and mix of uses
Parking behind or beside buildings
*unless located on "primary street"– see map page 9

T5

20-foot maximum front setback
18-foot minimum façade height*
5 stories max. if single use
8 stories w/ public space or mix of uses
10 stories w/ public space and mix of uses
Parking behind or beside buildings

-N

Neighborhood zone
No single-use non-residential

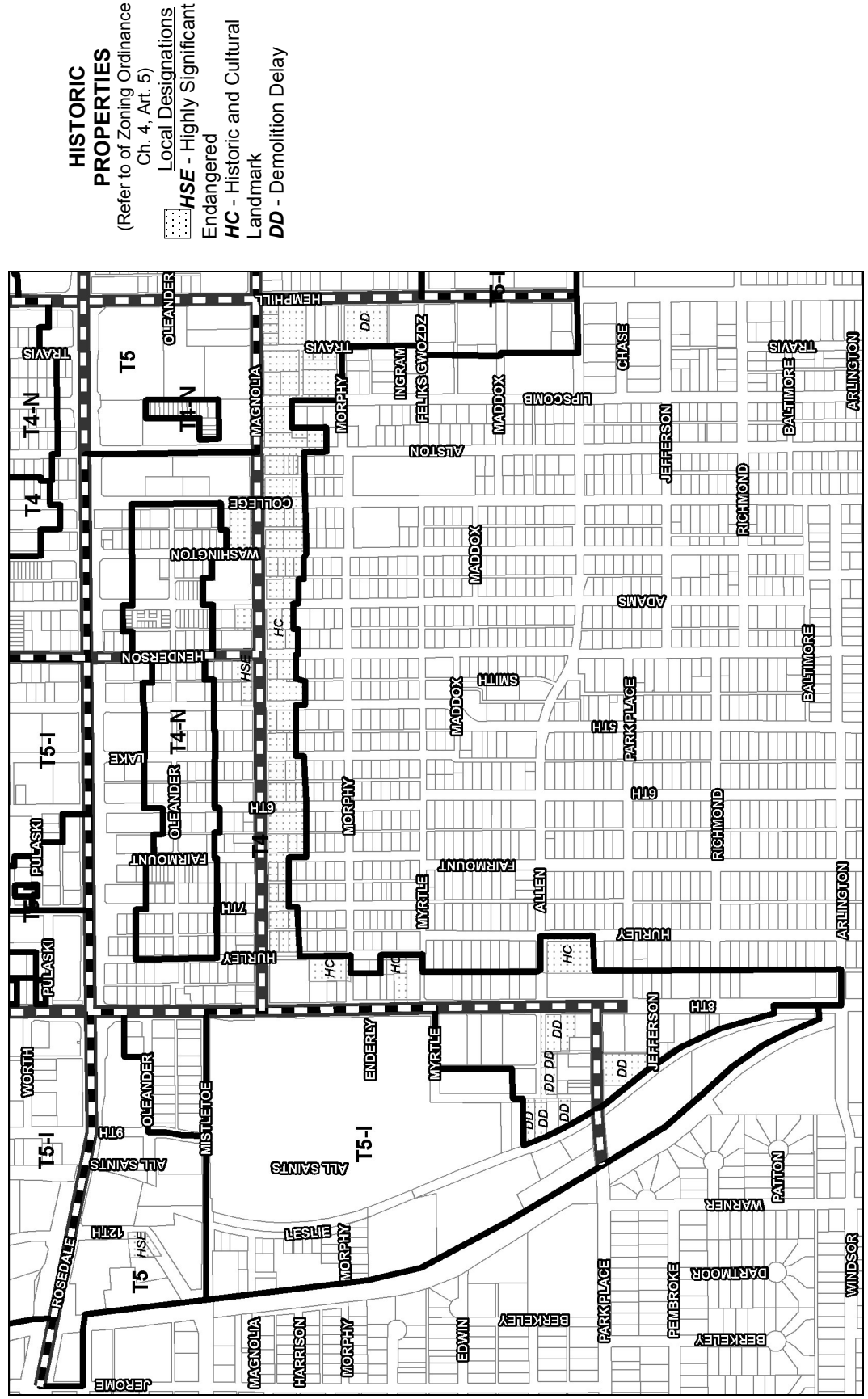
-R

Restricted zone
No bars or industrial uses;
see p. 44.

Institutional/Industrial zone
Flexible driveway and entrance standards

-I

SOUTHWEST QUADRANT



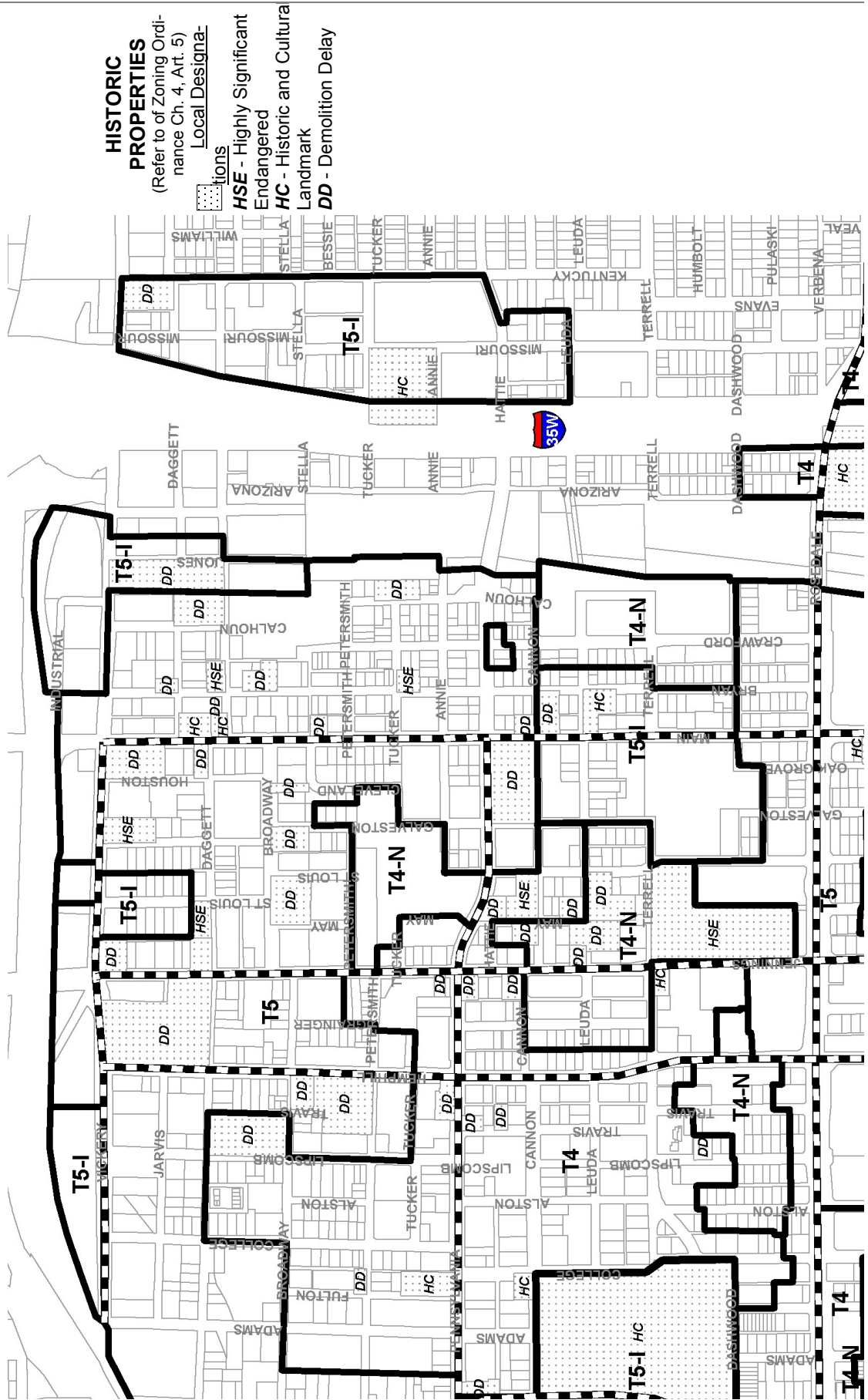
4.B. Development Zones and Historic Properties

3. NORTHEAST

Basic Development Standards (Standards and guidelines are listed in Sections 5-7.)

T4	<p>20-ft. maximum front setback</p> <p>18-foot minimum façade height*</p> <p>3 stories max. if single use</p> <p>5 stories max. w/ public space or mix of uses</p> <p>6 stories max. w/ public space and mix of uses</p> <p>Parking behind or beside buildings</p> <p>*unless located on "primary" street, see map page 9.</p>	T5	<p>20-foot maximum front setback</p> <p>18-foot minimum façade height</p> <p>5 stories max. if single use</p> <p>8 stories w/ public space or mix of uses</p> <p>10 stories w/ public space and mix of uses</p>	-N	<p><u>Neighborhood zone</u></p> <p>No single-use non-residential</p>	-R	<p><u>Restricted zone</u></p> <p>No bars or industrial uses; see p. 44.</p>
				-I	<p><u>Institutional/Industrial zone</u></p> <p>Flexible driveway and entrance standards</p>		

NORTHEAST QUADRANT



4.B. Development Zones and Historic Properties

4. SOUTHEAST

Basic Development Standards (Standards and guidelines are listed in Sections 5-7.)

T4

20-ft. maximum front setback
18-foot minimum façade height*
3 stories max. if single use
5 stories max. w/ public space or mix of uses
6 stories max. w/ public space and mix of uses
Parking behind or beside buildings
unless located on "primary street"– see map page 9

T5

20-foot maximum front setback
18-foot minimum façade height
5 stories max. if single use
8 stories w/ public space or mix of uses
10 stories w/ public space and mix of uses

-N

Neighborhood zone
No single-use non-residential

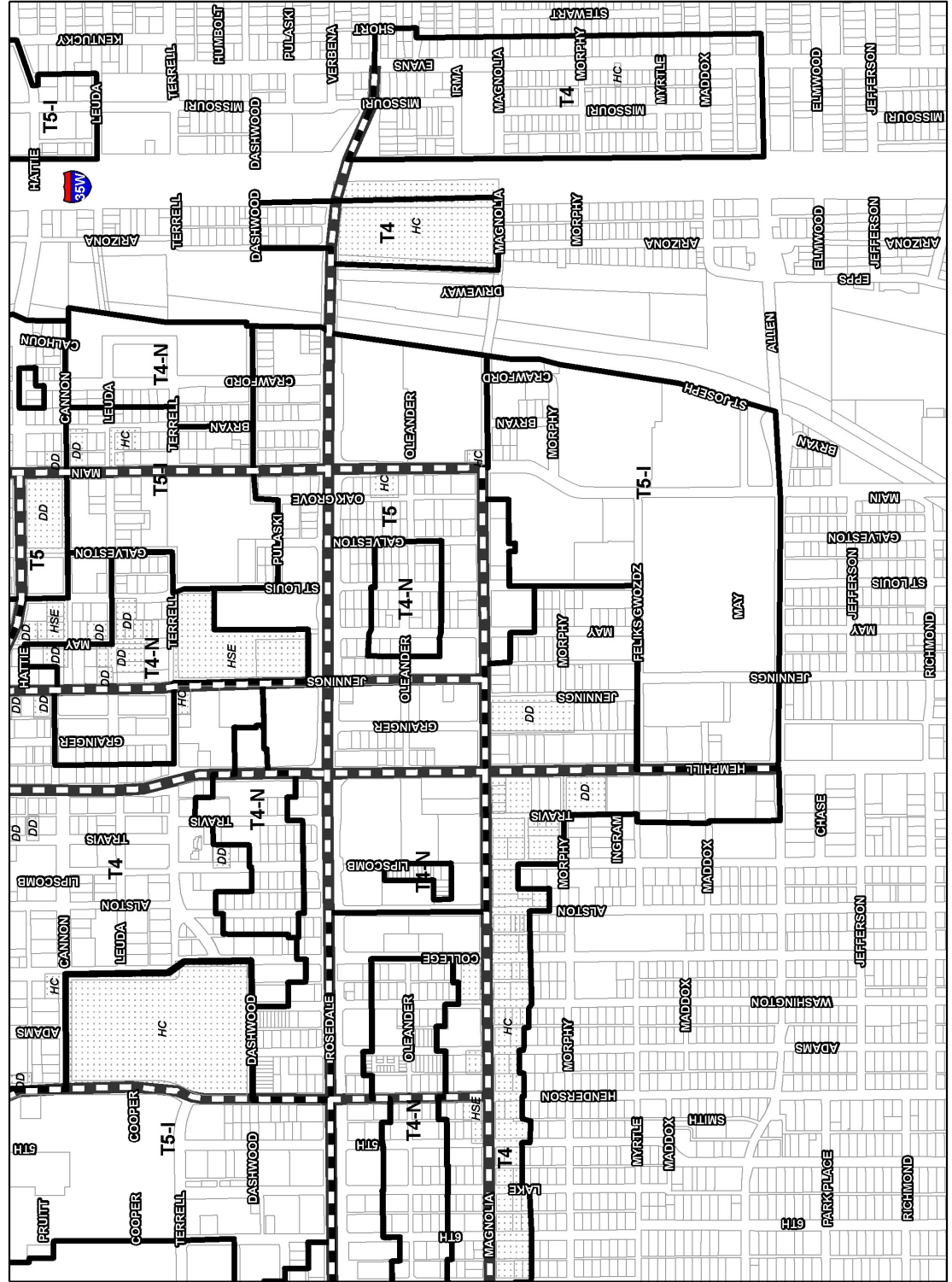
-R

Restricted zone
No bars or industrial uses;
see p. 44.

-I

Institutional/Industrial zone
Flexible driveway and entrance standards

SOUTHEAST QUADRANT



HISTORIC PROPERTIES

(Refer to of Zoning Ordinance Ch. 4, Art. 5)
Local Designations
HSE - Highly Significant Endangered
HC - Historic and Cultural Landmark
DD - Demolition Delay

4.C. Circulation Network and Thoroughfare Classification

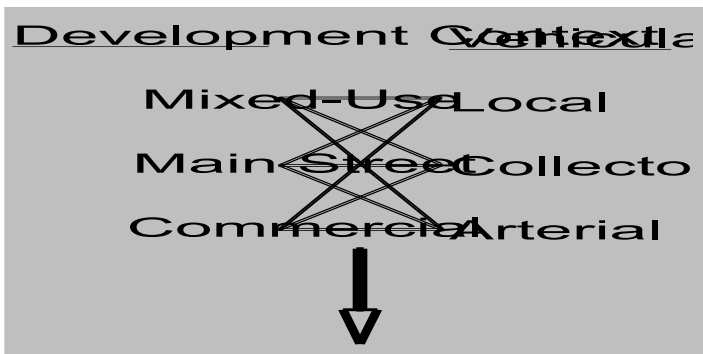
1. CLASSIFICATIONS BY CONTEXT: DEFINITIONS

- a. **Mixed-Use Streets** – Flexible and walkable urban streets serving a variety of land uses and densities. These include on-street parking and comfortable sidewalks, and are comfortable and safe for bicyclists.
- b. **Main Streets** – Walkable, mixed-use streets of moderate to high density. These are primary destination streets with active ground floor uses and on-street parking, and are comfortable and safe for bicyclists.
- c. **Commercial Streets** – Automobile-oriented streets with high levels of automobile and truck movement, no on-street parking, and low to moderate levels of pedestrian and bicycle movement.

2. CLASSIFICATIONS BY VEHICULAR CAPACITY: DEFINITIONS

- a. **Local Streets** – less than 2,500 vehicles per day.
 - b. **Collector Streets*** – less than 8,000 vehicles per day.
 - c. **Arterial Streets** – more than 8,000 vehicles per day.
 - Minor* – Usually undivided with 3-4 travel lanes
 - Major* – Usually divided with 4 travel lanes
 - Principal – Usually divided with 6 travel lanes
- *If feasible, striped bike lanes are usually necessary on these streets to create a safe and comfortable cycling environment.

3. CONTEXT-SENSITIVE STREET CLASSIFICATIONS



Context-Sensitive Street Classifications (Examples)

Mixed-Use Local (North-south streets at Oleander Walk)
 Mixed-Use Collector (Jennings north of Rosedale)
 Mixed-Use Arterial (8th Avenue)

Main Street Local (Park Place)
 Main Street Collector (Magnolia)
 Main Street Arterial (West Rosedale between 8th Ave. and S. Main)

Commercial Local (No Near Southside examples)
 Commercial Collector (Vickery across I-35W)
 Commercial Arterial (West Rosedale, west of 8th Ave. and east of S. Main)

CONTEXT-SENSITIVE CLASSIFICATIONS



Mixed-Use Streets are walkable and serve a variety of residential and non-residential land uses.



Main Streets are the primary retail and destination streets.



*Commercial Streets are oriented to vehicles but still provide comfortable pedestrian environments.
 (West Rosedale rendering by Carter & Burgess)*

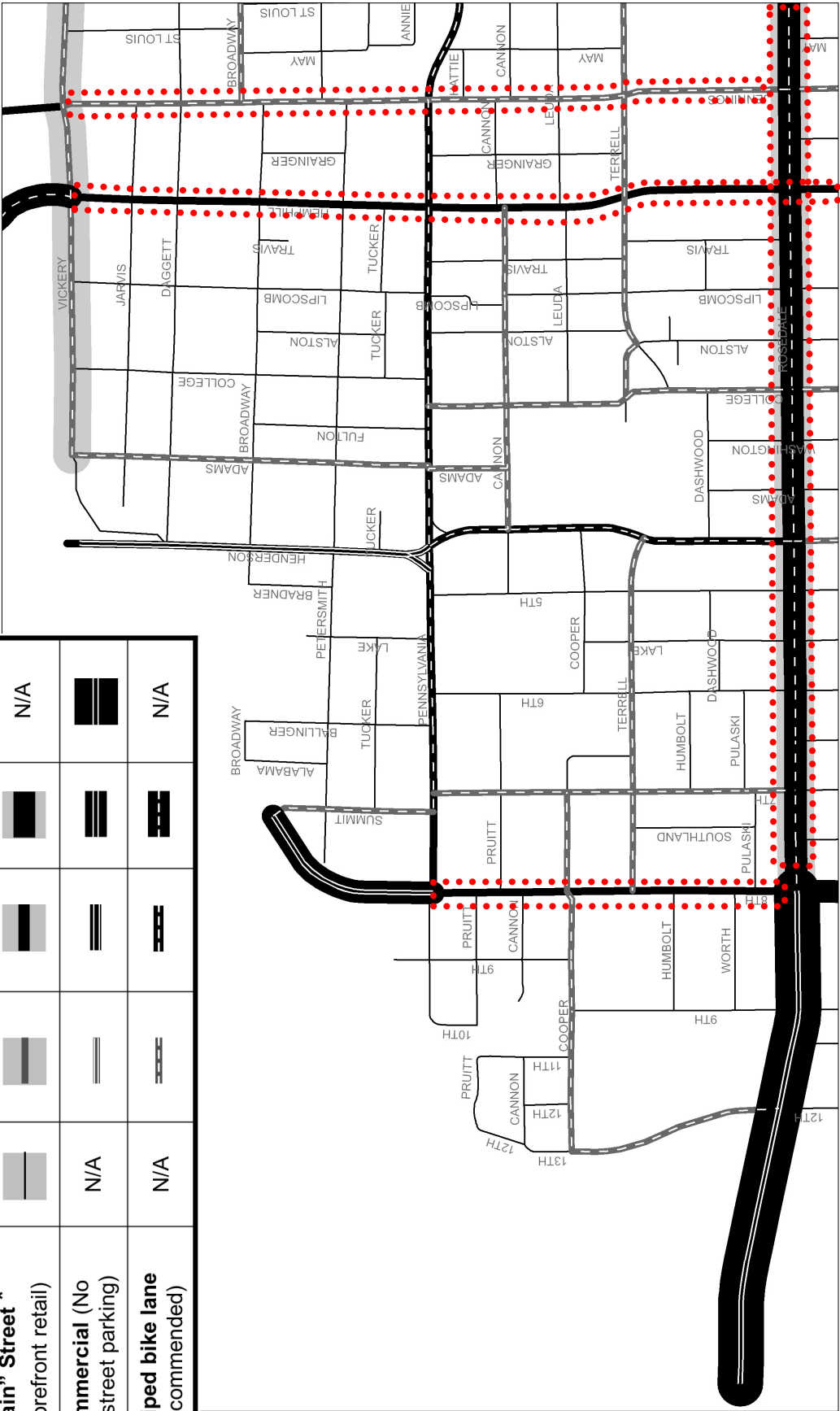
4.C. Circulation Network and Thoroughfare Classification

4. NORTHWEST QUADRANT

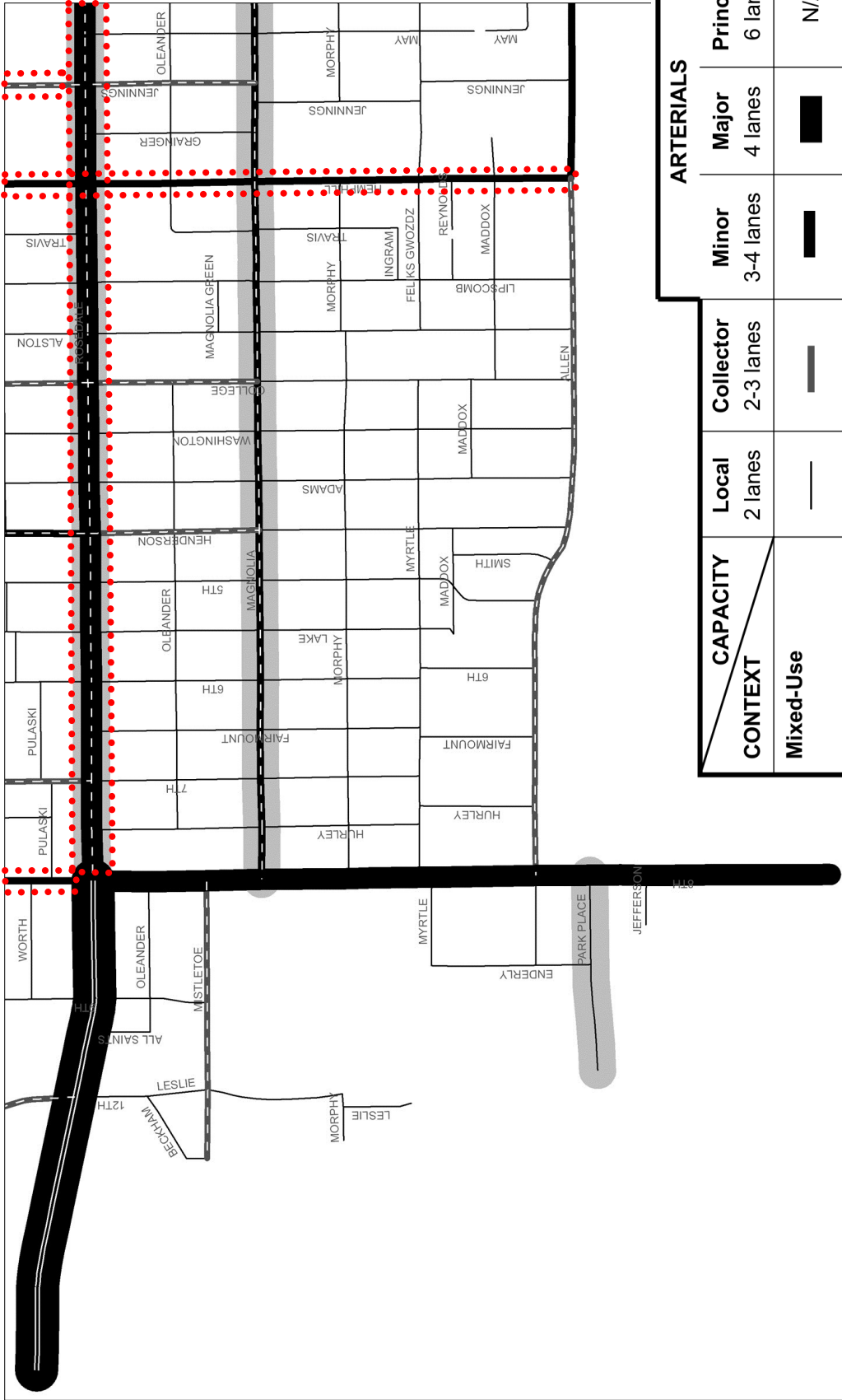
ARTERIALS				
CAPACITY CONTEXT	Local 2 lanes	Collector 2-3 lanes	Minor 3-4 lanes	Major 4 lanes
Mixed-Use	—	—	—	—
“Main” Street * (Storefront retail)	—	—	—	—
Commercial (No on-street parking)	N/A	—	—	—
Striped bike lane (Recommended)	N/A	—	—	—

* See: 5.B.2.b. 5.B.3.d. 5.C.2.a.
5.C.5.c. 5.E.8. 5.F.5.d.
5.F.5.f.

Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.



4.C. Circulation Network and Thoroughfare Classification
5. SOUTHWEST QUADRANT



* See: 5.B.2.b. 5.B.3.d. 5.C.2.a.
5.C.5.c. 5.E.8. 5.F.5.d.
5.F.5.f.

Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.

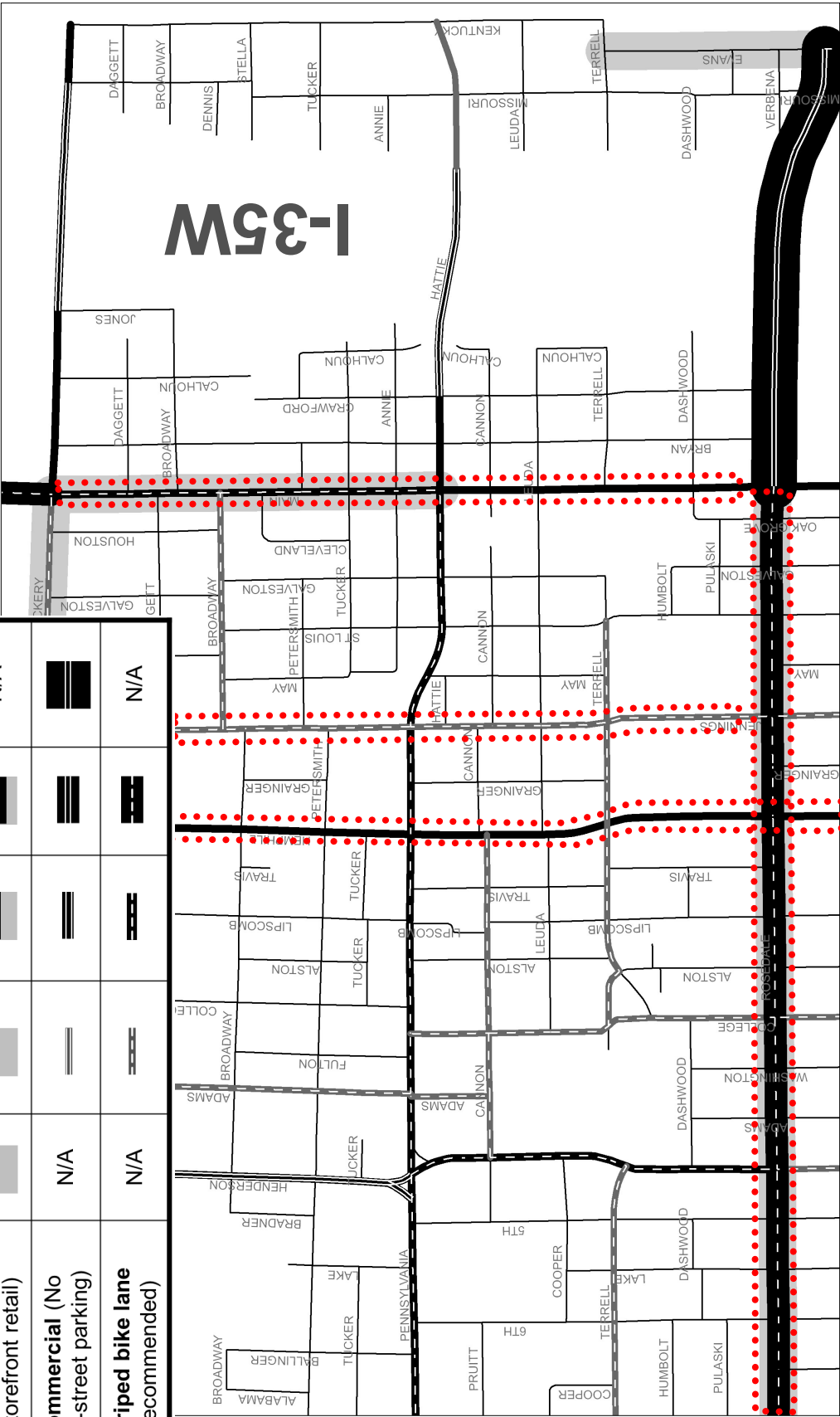
CAPACITY CONTEXT	ARTERIALS				
	Local 2 lanes	Collector 2-3 lanes	Minor 3-4 lanes	Major 4 lanes	Principal 6 lanes
Mixed-Use	—	—	—	—	N/A
“Main” Street * (Storefront retail)	—	—	—	—	N/A
Commercial (No on-street parking)	N/A	—	—	—	—
Striped bike lane (Recommended)	N/A	—	—	—	N/A

4.C. Circulation Network and Thoroughfare Classification
6. NORTHEAST QUADRANT

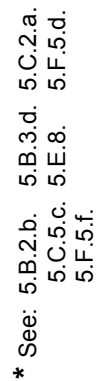
ARTERIALS					
CAPACITY CONTEXT	Local 2 lanes	Collector 2-3 lanes	Minor 3-4 lanes	Major 4 lanes	Principal 6 lanes
Mixed-Use	—	—	—	—	N/A
“Main” Street * (Storefront retail)	—	—	—	—	N/A
Commercial (No on-street parking)	N/A	—	—	—	—
Striped bike lane (Recommended)	N/A	—	—	—	N/A

* See: 5.B.2.b. 5.B.3.d. 5.C.2.a.
















Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.



7. SOUTHEAST QUADRANT



Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.

CAPACITY CONTEXT		ARTERIALS				
		Local 2 lanes	Collector 2-3 lanes	Minor 3-4 lanes	Major 4 lanes	Principal 6 lanes
Mixed-Use						N/A
“Main” Street * (Storefront retail)						N/A
Commercial (No on-street parking)		N/A				
Striped bike lane (Recommended)		N/A				N/A

(This page intentionally left blank)

5.A. Intent and Principles

This section includes standards and guidelines related to the orientation and configuration of streets, public spaces, buildings, sites, and parking facilities. The purpose of these standards and guidelines is to promote high quality, pedestrian-oriented, sustainable development that adapts to changing conditions over time, without dictating architectural style. The following principles provide the basis for the development standards and guidelines.

STREETS AND PUBLIC SPACES

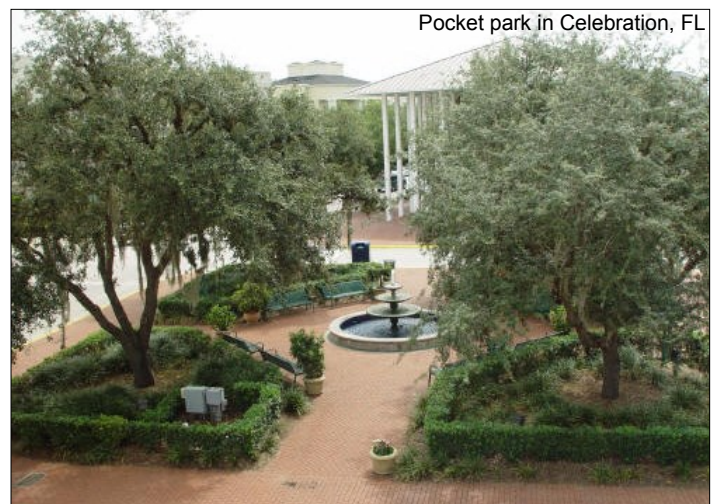
- Design all streets and sidewalks to promote pedestrian activity and comfort. Sidewalks serve as the Near Southside's most heavily used public spaces.
- Balance the circulation requirements of automobiles, mass transit vehicles, bicycles, and pedestrians in the design of street cross-sections. Utilize context-sensitive design strategies to achieve this balance.
- Provide designated road space for bicycles, such as striped bike lanes, on roads that would otherwise be uncomfortable or unsafe for less experienced or slow riders.
- Emphasize attention to detail and quality construction in the design and construction of streets.
- Use simple but effective roadside designs to allow easy maintenance.
- Provide on-street parking in as many areas as possible to support district businesses and to calm traffic speeds.
- Adhere to time-tested roadside design strategies that create walkable streets, including shade trees and pedestrian lights located along the curb, between the roadway and the walkway.
- Provide a wide range of public spaces (in addition to comfortable sidewalks), including neighborhood-oriented pocket parks, community gathering places, and recreational facilities.
- Incorporate elements into public spaces that engage all age groups, including young children and the elderly.



On-street parking, wider sidewalks, street trees, and outdoor dining areas will help create the district's most appealing and



On-street parking supports retail and restaurants and also provides a traffic buffer for pedestrians.



A network of public spaces that includes both smaller plazas and larger parks is a key element in urban districts.

5.A. Intent and Principles (cont.)

SITES AND BUILDINGS

- Design sites and buildings, and the lower floors of buildings in particular, so that they promote pedestrian activity and provide an active, human-scale, continuous, pedestrian-oriented street edge along public sidewalks.
- Design buildings and sites that provide visual variety and enhance the Near Southside's overall sense of place.
- Build distinctive neighborhoods that enhance local identity.
- Promote building scale that enhances the public realm, and promote new buildings that complement the scale of neighboring structures, particularly adjacent historic buildings.
- Locate and design surface parking lots, individual garages, and large parking garages so as to reduce their visual impact and/or to create public amenities (e.g., through aesthetic or environmental features).
- Design and orient buildings on Magnolia, Park Place, West Rosedale, South Main, Pennsylvania, 8th Avenue, and Vickery so as to support the streets' roles as the Near Southside's primary pedestrian-oriented commercial districts.
- Encourage the use of durable, attractive materials that promote sustainability and the reuse of buildings.
- Design buildings that reflect the time in which the structures are built, as the district's most treasured historic buildings do.
- Without dictating architectural style, encourage building facade designs that contribute to an attractive streetscape and skyline.
- Encourage all civic and publicly sponsored buildings to be of exceptional design quality.
- Design buildings and sites so as to minimize negative impacts on air and water quality and to promote a sustainable natural environment.



Storefronts along 8th Avenue

The principles support active streets with ground level uses that are oriented to public sidewalks .



Parking lot located to the side of a historic building on S. Main St.

Parking lots located behind or to the side of buildings are consistent with the district's urban character.



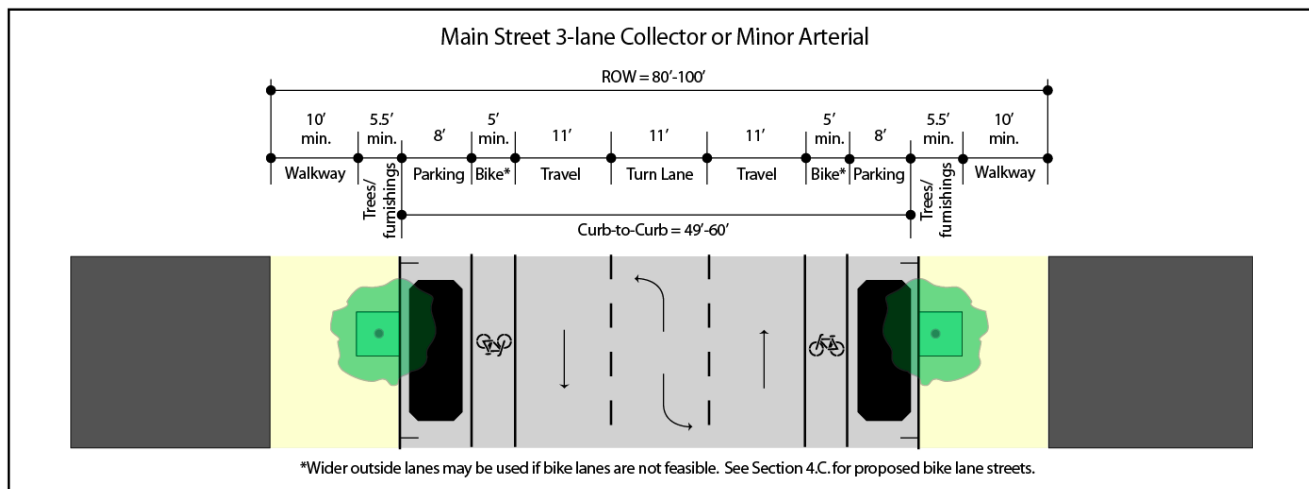
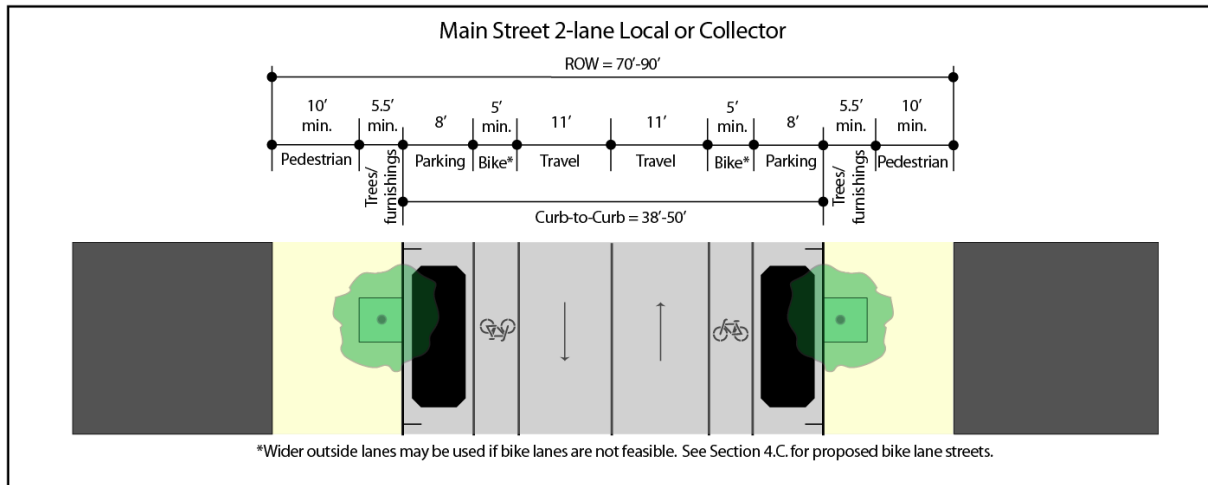
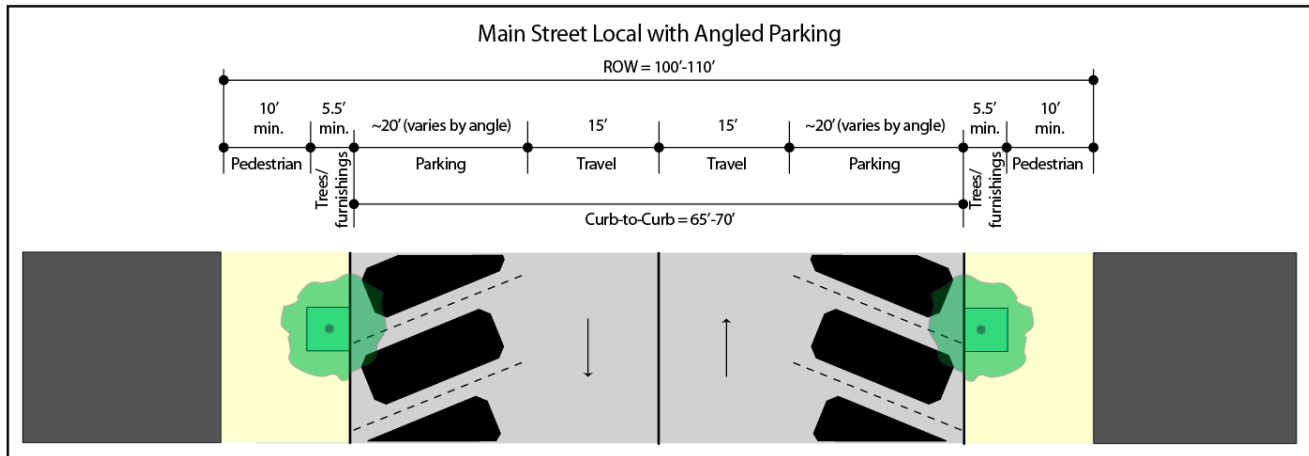
Historic Mehl Building on Magnolia Avenue

Historically significant buildings help define the district and should be incorporated into new projects.

5.B. Streets and Other Public Spaces

1. STREET STANDARDS

a. Main Streets



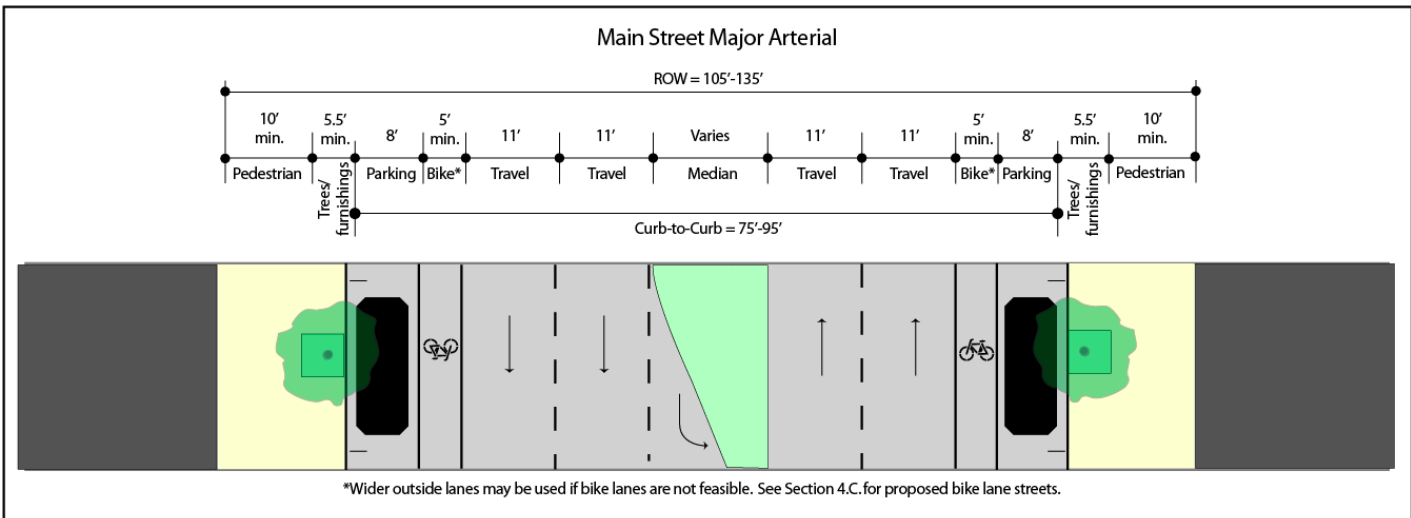
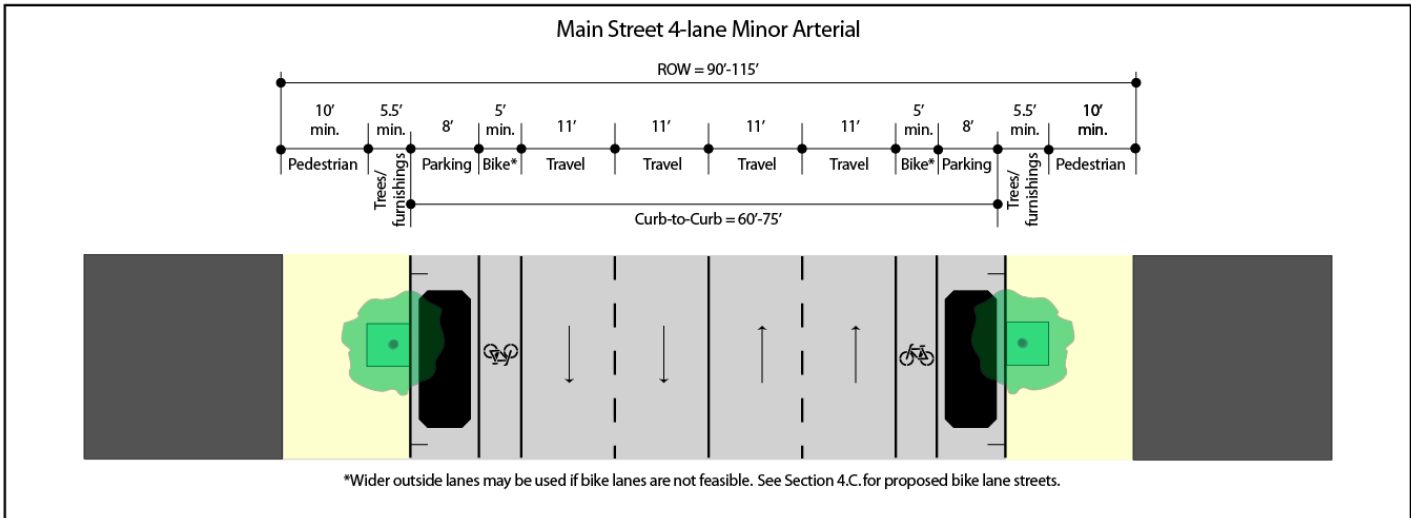
NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

a. Main Streets (cont.)



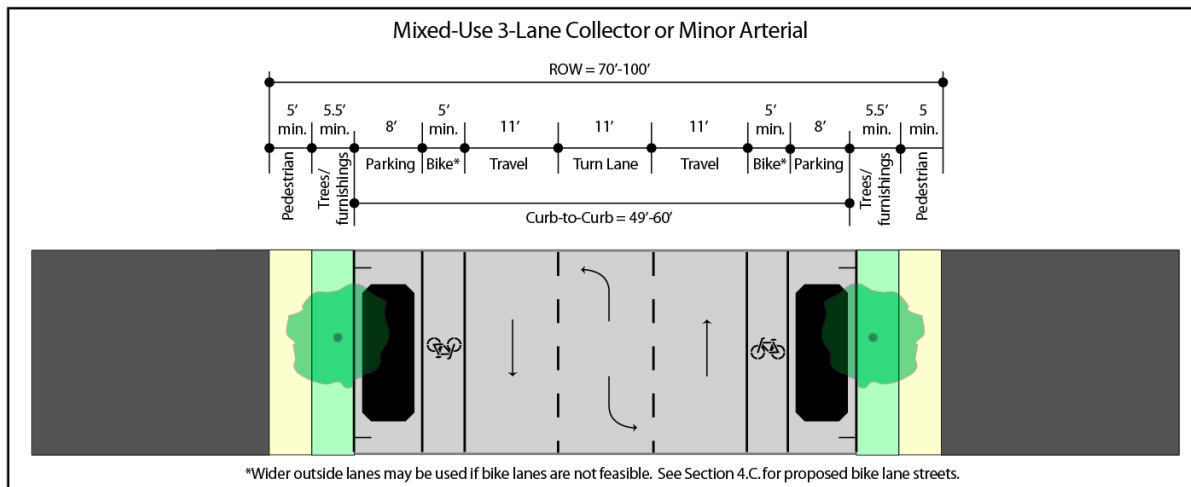
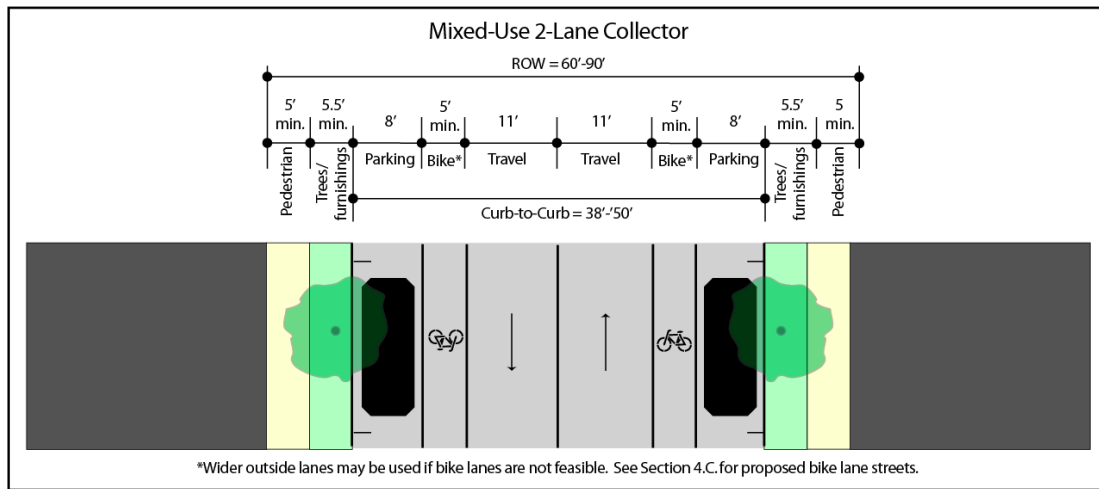
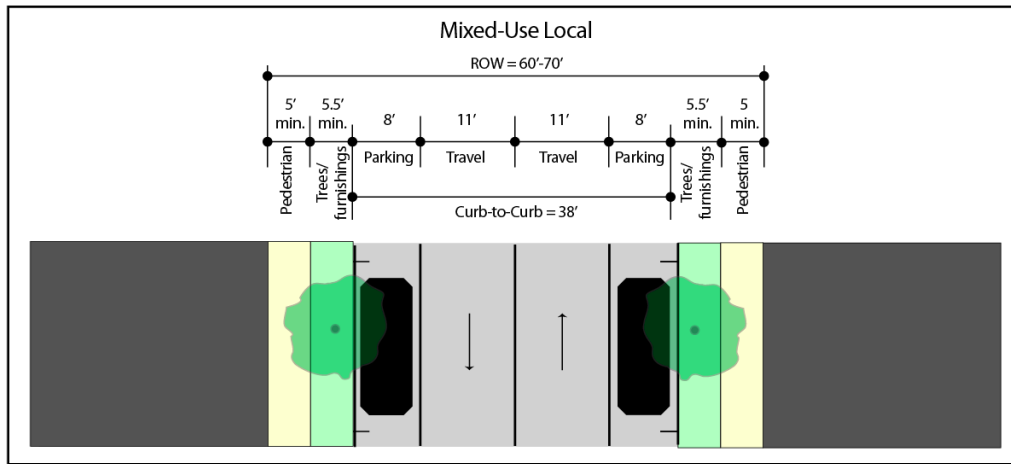
NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

b. Mixed-Use Streets



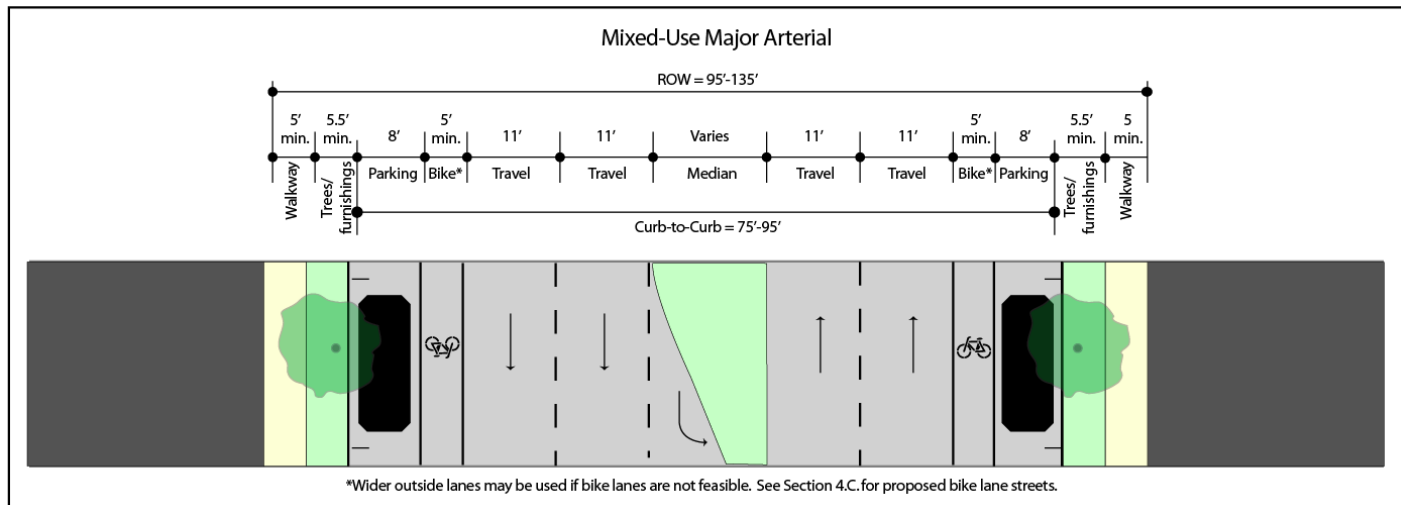
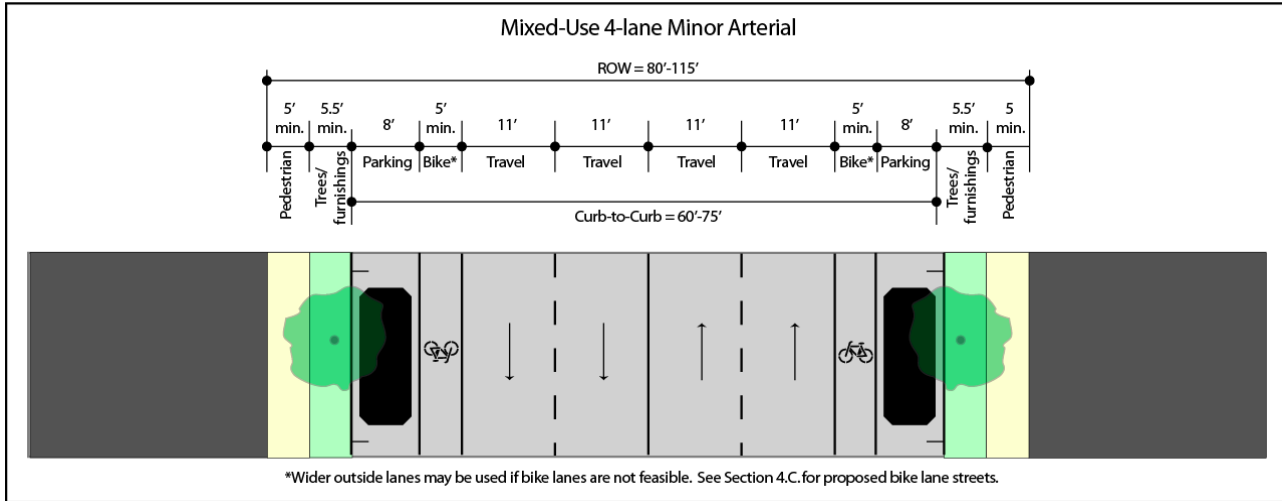
NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

b. Mixed-Use Streets (cont.)



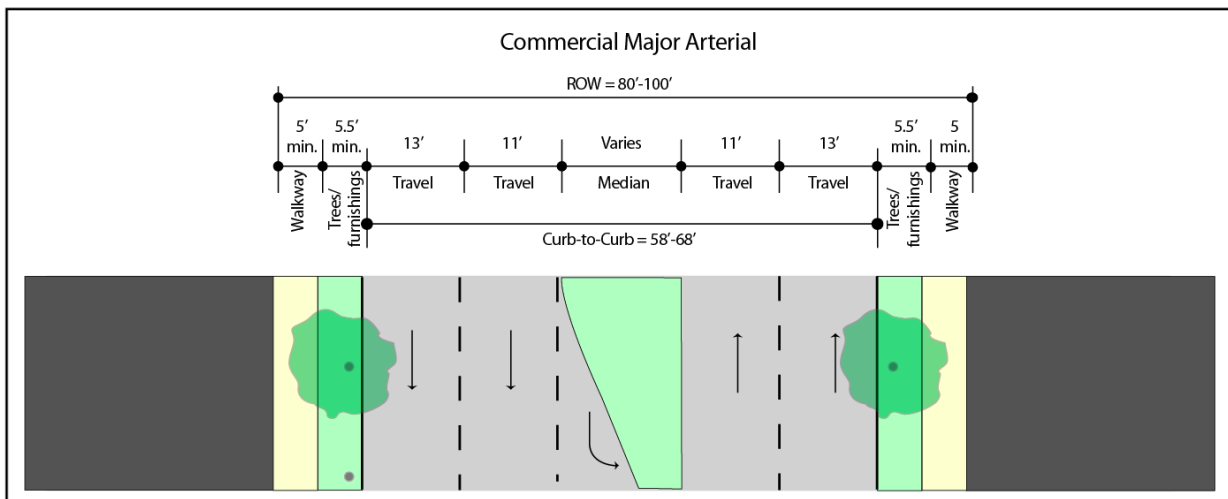
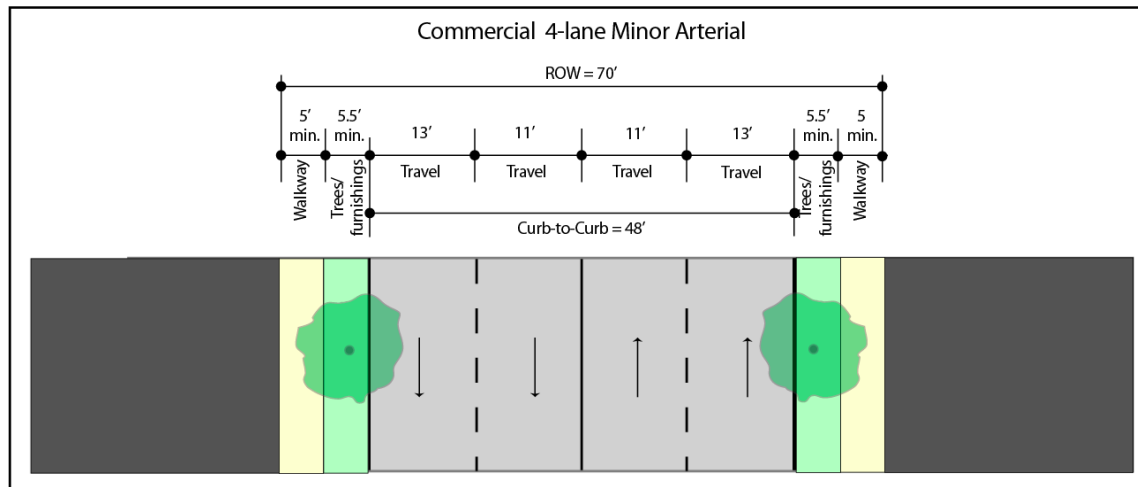
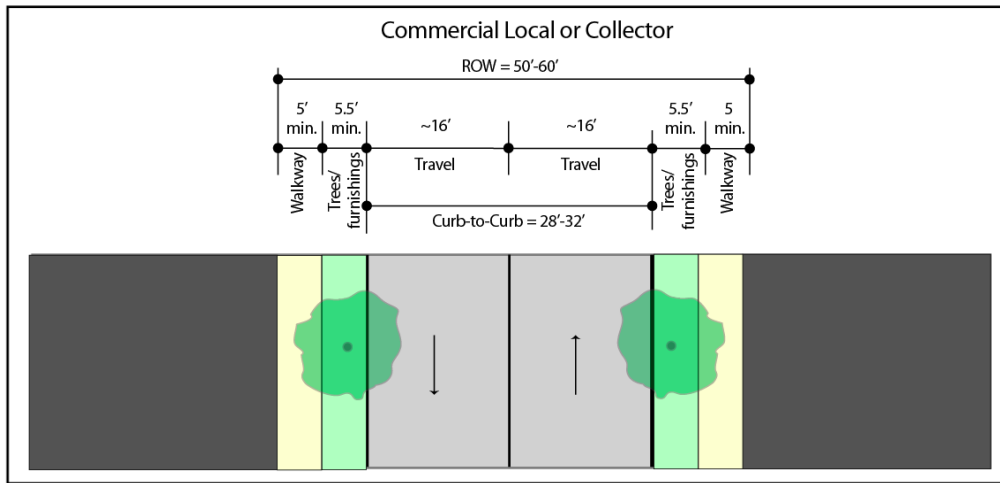
NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

c. Commercial Streets



NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)**1. STREET STANDARDS****d. Alleys**

1) Alleys shall conform to all standards contained in the Subdivision Ordinance. Chapter 6, Article VI, Section G.2. of the Subdivision Ordinance includes the following dimensional requirements:

- Alleys serving single-family (including townhouses) or two-family residential:

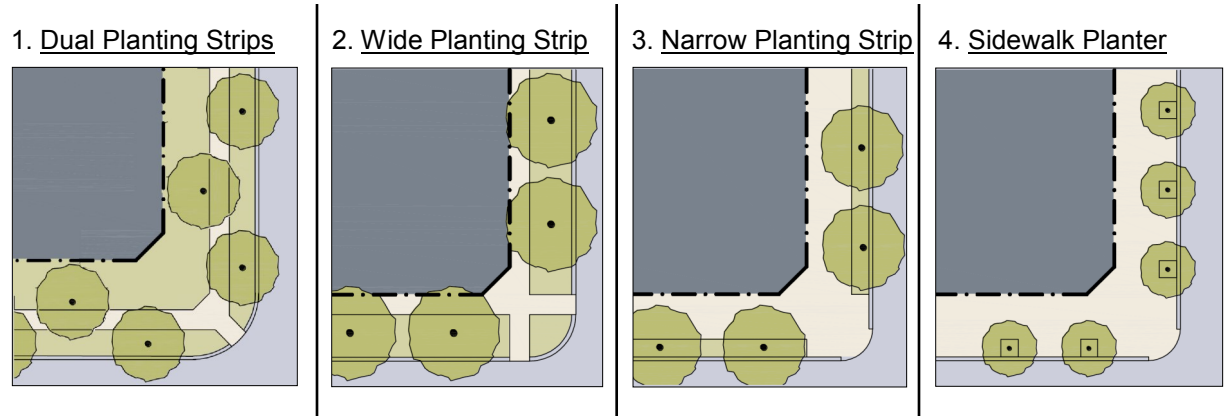
Right-of-way width: Minimum 16 feet

Pavement width: Minimum 12 feet

- Alleys serving other uses:

Right-of-way width: Minimum 20 feet

Pavement width: Minimum 20 feet

5.B. Streets and Other Public Spaces (cont.)**2. PUBLIC ROADSIDES****a. Roadside Design Options**

Suitable Streets – Listed in order of applicability; see Thoroughfare Classification Maps in section 4.C.

•Commercial Streets •Mixed-Use Streets	•Mixed-Use Streets •Commercial Streets	•Mixed-Use Streets •Main Streets	•Main Streets •Mixed-Use Streets
---	---	-------------------------------------	-------------------------------------

Roadside Width – Total width of pedestrian way and street tree/furniture zone (see 5.B.2.b. for minimums).

~20+ feet	~13-15 feet	~10-15 feet	~10-20 feet
-----------	-------------	-------------	-------------

Pedestrian Way

Paved walkway	5 ft. minimum, (6 ft. minimum if adjacent to angled parking)	5 ft. minimum, (6 ft. minimum if adjacent to angled parking)	5-10 ft., (6 ft. minimum if adjacent to angled parking)	10-15 ft.
Walkway location	Between planting strips	Along property line	•Along property line, or •Along planting strip if sidewalk seating or display is located along property line	•Along property line, or •Along planting strip if sidewalk seating or display is located along property line

Street Tree/Furniture Zone – Located along the curb; includes street trees, pedestrian lights, and other elements.

Planter	Min. 5.5 ft. wide (each)	Min. 8 ft. wide	Min. 5.5 ft. wide	Min. 5 ft. x 5 ft. tree well
Tree spacing (Sec. 3.a)	25-35 feet (Must be ≥10' from bldg.)	15-35 feet	15-35 feet	15-35 feet
Pedestrian light spacing (Sec. 3.c)	60 feet maximum	60 feet maximum	60 feet maximum	60 feet maximum
Other elements (Guidelines in Sec. 3.d.)	•Irrigated groundcover •Fire hydrants	•Irrigated groundcover •Fire hydrants	•Irrigated groundcover •Bike racks •Trash bins •Benches •Wayfinding signs •Sidewalk tables and displays	•Bike racks •Trash bins •Benches •Wayfinding signs •Sidewalk tables and displays

5.B. Streets and Other Public Spaces (cont.)**2. PUBLIC ROADSIDES (cont.)**

- b. **Projects subject to roadside standards** – All new construction projects, as well as major renovations in which the:
1. Gross floor area to be renovated exceeds 10,000 sf, and
 2. Value of the renovation exceeds the appraised value (per Tarrant Appraisal District records) of the property's improvements prior to renovation.
- c. **Minimum roadside widths for new construction projects** – To accommodate the required minimum widths for street tree/furniture zones and pedestrian ways, minimum roadside width standards apply. These standards apply regardless of the available public right-of-way. Where insufficient right-of-way exists, the pedestrian way shall extend onto private property, with a public sidewalk easement provided. The roadside shall be measured from the back of the curb to the outside edge of the pedestrian way (closest to the building façade).
1. "Commercial" and "Mixed-Use" streets – 10.5 ft. minimum
 2. "Main" streets – 15 ft. minimum

STREET TREE SPECIES**Best**

Small Canopy
(Min. 15' spacing)
Redbud
Mexican Plum
Eve's Necklace
Medium Canopy
(Min. 25' spacing)
Caddo Maple
Bald Cypress
Large Canopy
(Min. 35' spacing)
Texas Red □ Oak
Shumard Oak
Live Oak
Durand Oak
Lacebark Elm
Cedar Elm

Good

Ginkgo (*Med.*)
Pecan (*Large*)

Prohibited

Hackberry
Sycamore
Silver Maple
Mulberry
Siberian Elm
Mimosa
Arizona Ash
Cottonwood
Willow
Bradford Pear
Chinese Pistache

3. ROADSIDE ELEMENTS**a. Street trees**

1. Developments must conform to the tree preservation standards set forth in Section 5.G.1. Canopy of street trees can be applied towards mitigation requirements. The provisions below also apply.
2. Public and private development shall provide shade trees within the street tree/furniture zone.
3. Street trees shall be a minimum of three (3) inch caliper and placed 15-35 feet apart on center, depending on the trees' size at maturity.* (See the list at right for spacing standards, and the spacing note on the bottom of p. 33.) Where necessary, spacing exceptions may be made to accommodate mature trees, curb cuts, fire hydrants and other infrastructure elements.
4. To maximize survival rates, trees shall be planted to ensure proper drainage and shall be irrigated.
5. Low maintenance tree planter systems, such as pervious pavers or planting strips, shall be used. Tree grates collect trash and damage trees and are not a low maintenance system.
6. Street trees shall be pruned to allow 8-foot clearance for pedestrians.
7. Trees shall be planted at the time of development, although the planting schedule may be extended to allow planting during the fall/winter season.

b. Pedestrian Way

1. The pedestrian way shall have walkways a minimum width of 5 feet and shall comply with ADA and Texas Accessibility Standards. Exemption: Minor renovation projects on properties with existing 4 ft. sidewalks.
2. Paving materials shall be attractive and easy to maintain. Appropriate materials:
 - Concrete
 - Pervious concrete
 - Masonry pavers over concrete base
 - Similarly durable materials that meet City standards

ROADSIDE ZONES

*Street tree/
furniture zone*

- Trees
- Lights
- Bike Racks
- Tables

Pedestrian way

- Clear walkway
- Awnings and other shade elements

5.B. Streets and Other Public Spaces (cont.)**3. STREET LANDSCAPING AND FURNISHINGS (cont.)****c. Pedestrian Lights**

1. New public and private development shall provide pedestrian lights within the roadside's street tree/furniture zone.
2. Pedestrian lights shall be located approximately at the midpoint between two trees and spaced a maximum of 60 feet apart.* (See note below.) Where necessary, spacing exceptions may be made to accommodate mature trees, curb cuts, fire hydrants and other infrastructure elements.

3. Guidelines

- Continuity of lighting style throughout a neighborhood is encouraged.
- Lights should be durable and easy to maintain or replace.

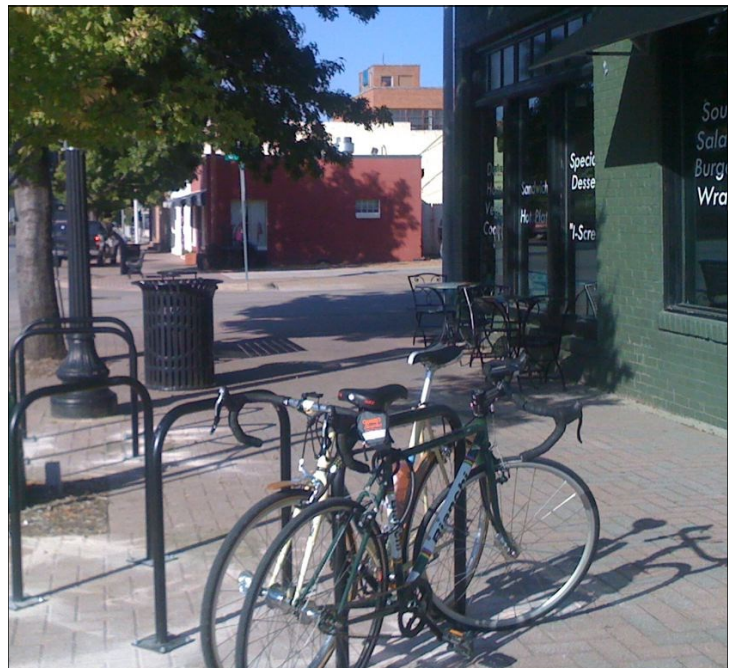
d. Guidelines for other roadside elements

1. Bike racks, trash bins, and seating should be incorporated into streetscape designs on "Main Streets" and other streets with high levels of pedestrian activity. Continuity of style throughout a neighborhood is encouraged. These elements should be durable, cost effective, and easy to maintain.
2. Transit shelters should:
 - Be located in areas with a high level of pedestrian activity, but shall not impede pedestrian traffic or wheelchair circulation.
 - Provide comfortable seating and should not use the standard, stand-alone advertising benches seen at many bus stops.
 - Where feasible, incorporate other streetscape elements, such as water fountains, newspaper boxes, trash bins, and/or signage.
 - Reflect design excellence and should be designed as more than simple utilitarian fixtures. Creative and timeless designs that complement other streetscape furnishings are encouraged.
 - Be constructed of durable, attractive, and sustainable materials that are easy to maintain.

* If no pedestrian lights or street trees exist on the project's block face, measure from the end of the block to determine spacing. (For trees: begin 40 feet from curb intersection to accommodate public open space easement requirement.) If trees or lights exist, measure from existing trees or lights.

PEDESTRIAN LIGHTS

Since its inception, Fort Worth South, Inc. has worked with developers to install pedestrian lights as part of new construction projects, including this development along College Avenue. The roadside standards in this section promote the installation of lights throughout the Near Southside.

BIKE RACKS

Bike racks that are easy to recognize, attractive, and simple to use help make the Near Southside bike-friendly, providing convenience and security.

5.B. Streets and Other Public Spaces (cont.)

4. PUBLIC PARKS AND PLAZAS

- a. **Park Dedication Policy** – The City's park dedication policy applies to new residential units.
- b. **Plazas** – Plazas are urban public spaces that are more formal than parks and have a higher degree of hard surfaces and pedestrian traffic. Guideline for impervious surface area: Maximum 40 percent.
- c. **Parks** – Parks are open spaces that have less hard surface and pedestrian traffic than plazas. Parks typically include both active and passive recreation areas. Guideline for impervious surface area: Maximum 20 percent.
- d. **Hours** – Publicly and privately maintained public spaces shall be publicly accessible during daylight hours. Access may be limited at other times.
- e. **Design guidelines** – Publicly and privately developed public spaces should conform to the following guidelines, to the extent possible.
 1. Design all public spaces other than large recreational parks to be human-scale and visually interesting.
 2. Locate and design public spaces so that they are clearly visible and easily accessible.
 3. Incorporate space-defining and active edges – such as multi-story façades with ground floor restaurants – but avoid blank walls. Cafés and shops that attract pedestrians are ideal edge uses.
 4. Plan for prevailing sun angles and climatic conditions.
 5. Avoid locating public spaces on corner lots. Corners are better suited for buildings that establish a strong urban street edge.
 6. Avoid sunken plazas and architectural bench arrangements; these often fail to promote use.
 7. Provide flexible space for programmed uses, but design such spaces so that they do not appear barren when there is no programming.
 8. Incorporate features that attract users, such as fountains, public display areas, or interactive sculpture.
 9. Incorporate landscaping to provide natural shade and/or to soften hardscape areas.

5. PUBLIC ART GUIDELINES

- a. **Public art in private projects** – Where feasible, developers are encouraged to integrate art into the design process for buildings and public spaces.
- b. **Public art in public projects** – Public agencies are encouraged to integrate art in the design of all publicly visible infrastructure, including retaining walls and public spaces.
- c. **Fort Worth Art Commission assistance** – The Fort Worth Art Commission may assist with identifying appropriate artists and/or advise on selection processes for projects.

PUBLIC SPACES



The standards and guidelines encourage a variety of public spaces on the Near Southside, ranging from the type of active urban plaza shown above to more passive and heavily landscaped neighborhood parks.

PUBLIC ART



The firefighter boots sculpture at Fire Station #8 was funded through the City's public art program. In addition to art that is integrated into City projects, privately sponsored public art can greatly enhance a development project's appeal and value.

5.C. Building Location and Orientation

1. BASE SETBACK STANDARDS

New buildings shall be located in relation to the boundaries of their lots according to the setback standards listed below and depicted to the right.

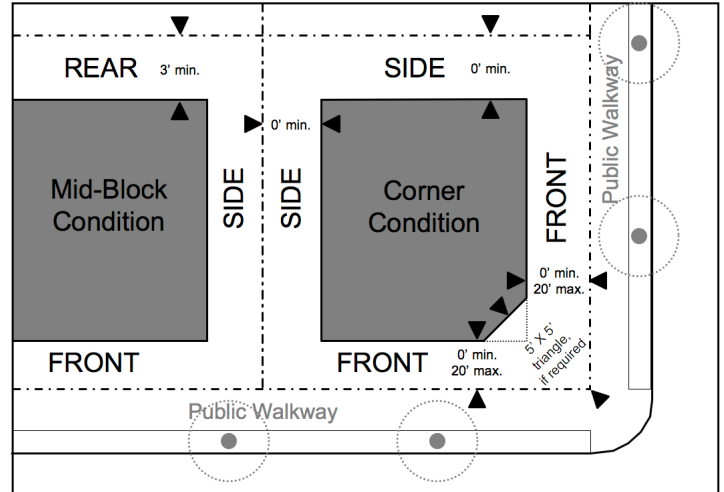
Front Setback	0 ft. min., 20 ft. max. except as described in Section 5.C.2. below.
Side Setback	0 ft. min., except as described in Section 5.C.2.e. below.
Rear Setback	3 ft. min.
Corner Set-back	5' x 5' dedication when required by Section 5.C.2.f. below.

General yard development standards: Development shall be exempt from Chapter 6, Article 1, Sections 6.101B, 6.101C, 6.101E, and 6.101G of the Zoning Ordinance.

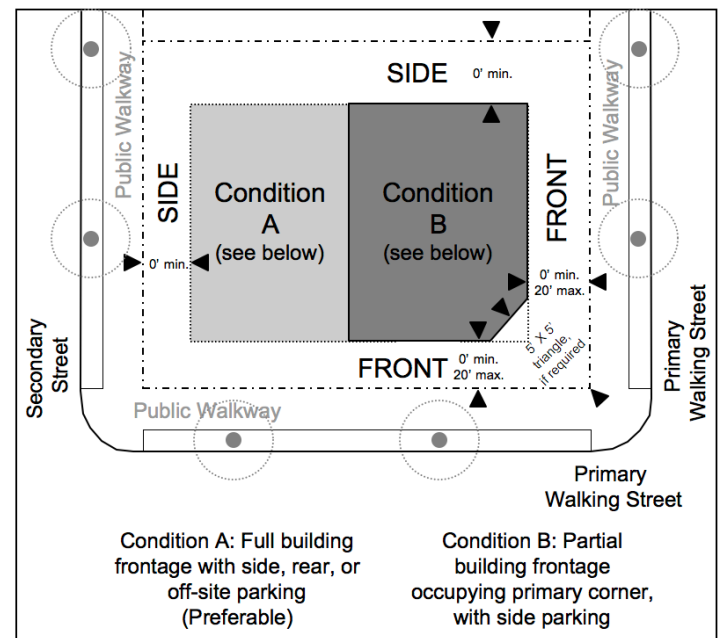
2. CONTEXTUAL SETBACK STANDARDS

- Buildings fronting designated “Main” streets** – Front setback is 0 ft. min. and 10 ft. max. (Street classification maps shown in Section 4.C.)
- Buildings fronting public spaces** – Buildings that front pocket parks, plazas, or other public spaces may exceed the maximum front setback.
- Oleander Walk T4-N zone** – Front setback is 5 ft. min. and 10 ft. max. along Oleander Walk; 0 ft. min. and 10 ft. max. along other streets.
- “On-street” parking or public walkways located on private property** – If angled, perpendicular, or parallel parking is located on private property but functions the same as on-street parking, the front setback is measured from the edge of the public walkway. Similarly, if a required public walkway (see Section 5.B.2) encroaches onto private property, the setback is measured from the walkway edge as shown in diagrams to right.
- Side yards adjacent to existing buildings** – When an abutting property with an existing building has windows facing to the side, any new building shall provide at least 10 feet of separation.
- Corner buildings** – For corner buildings at street and alley intersections that do not include all-way stop signs or traffic signals, a triangular dedication measuring 5' by 5' is required by the subdivision ordinance. The triangle shall be measured from the property line and clear of visibility obstructions.
- Interior buildings** – Interior buildings may be constructed if the project also includes frontage buildings that are oriented to public streets. Specifically, buildings may exceed the setback if at least 60 percent of the public street frontage on each block face within the development contains buildings within the maximum setback of 20 ft. The frontage percentage is reduced to 50 percent for institutional or industrial campuses in “I” zones.

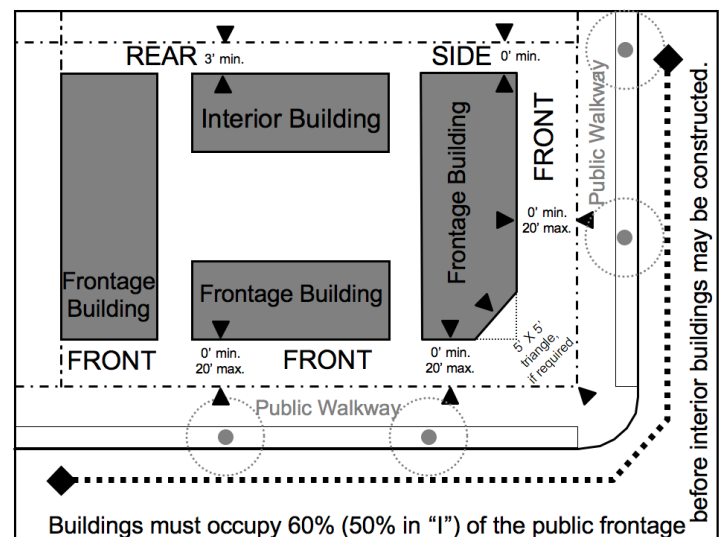
SETBACKS: PARTIAL BLOCKFACE CONDITIONS



SETBACKS: FULL BLOCKFACE CONDITIONS



SETBACKS: INTERIOR BUILDINGS



5.C. Building Location and Orientation (cont.)

3. PEDESTRIAN ENTRANCES

- a. **Primary pedestrian building entrances** shall be located on the street frontage of the building. If a site's slope presents significant impediments to a street fronting entrance, such as a need for stairs, within the public right-of-way – and a proposed entrance from an adjacent side façade would provide a similarly visible and inviting front entrance, City staff may approve an exception administratively. For buildings fronting other public spaces, the primary pedestrian entrance shall be oriented to and accessible from the public space.
- b. **Individual retail entrances** – Each retail use with exterior ground level exposure along a street or public space shall have an individual public entry from the street or public space.
- c. **Corner building guideline** – Entrances to corner buildings with ground floor retail uses should be located at the corner.

4. FENCING

- a. **Front yard perimeter fencing prohibited** – Conventional gated complexes with perimeter security fencing along public streets are prohibited. Specifically, exterior security fences and gates that are located along public streets, along private streets or walkways that are publicly accessible through a public use easement, or along publicly accessible open space shall not extend beyond building façades; i.e., these fences shall not be located in the area between building façades and the property line.
- b. **Privacy fences** – A front yard fence or railing not exceeding 4 feet in height may extend beyond building façades if the fence encloses a private patio, yard, or sidewalk eating area. The fence is not permitted to encroach on a required pedestrian walkway (see Section 5.B.3.b.).

5. DRIVE-THROUGH DESIGN STANDARDS

- a. **Guideline** – Conventional drive-through uses do not support a pedestrian-oriented environment and are inconsistent with the urban redevelopment goals for the Near Southside. Although drive-through facilities are discouraged, the following standards shall apply to facilities approved by the UDC.
- b. **Windows and stacking lanes** – Drive-through windows and stacking lanes shall not be located within the front yard setback or along façades that face a street; where possible, they shall be located to the rear of buildings.
- c. **Circulation** – The design and location of the facility shall not impede vehicular traffic flow and shall not impede pedestrian movement and safety. Driveways shall not be located on Main streets, such as Magnolia Avenue and South Main Street (see classification maps in Section 4.C.). Shared driveways and/or driveways located off of non-arterial streets should be used, where possible.
- d. **Screening** – Architectural elements, landscaping, and/or other screening elements shall be used to minimize the visual impacts of the drive-through facility.

PEDESTRIAN ENTRANCES



Medical office building
at Magnolia Green

Pedestrian-oriented projects orient primary entrances to public sidewalks, and corner retail/service buildings often include corner entrances.

FENCING



Conventional gated complexes are not allowed, but short privacy fences that clearly delineate private yards and patios are compatible.

URBAN DRIVE-THROUGHS



The well-screened Wells Fargo drive-through facility is a good example of a compatible design that fits into a walkable environment.

5.C. Building Location and Orientation (cont.)**6. SKY BRIDGES**

Sky bridges spanning public streets to connect upper floors of opposing buildings often negatively impact street-level pedestrian activity. There may be, however, unique circumstances that would allow for a sky bridge that serves a vital connectivity function without negative street-level impacts. All sky bridge proposals require Urban Design Commission review. To receive UDC approval, a proposed sky bridge must meet all of the following criteria:

- a. The sky bridge must serve a clear and vital connectivity function that is clearly impractical to achieve through a street level connection.
- b. The sky bridge must not remove significant pedestrian activity from street level, and the evaluation of potential activity lost should take into account both the potential number of pedestrians lost and any potential lost benefits stemming from pedestrians in that particular area, such as lost benefits to neighborhood safety or neighborhood businesses.
- c. The sky bridge must not visually obstruct significant view corridors.

SKY BRIDGES

UDC Approval is required for all sky bridges.

5.D. Building Height

1. HEIGHT GUIDELINE – New single-story buildings are strongly discouraged. Multi-story buildings are consistent with the urban character of the district and are essential in achieving economic and urban design goals for the district.
2. BUILDING HEIGHTS shall conform to the standards listed below and shown in the diagrams to the right. Zones are shown on maps in Section 4.B. Height is measured in stories, not including a raised basement or inhabited attic; i.e., the number of complete stories between the average grade of the frontage line to the eave of a pitched roof or to the surface of a flat roof.

Minimum heights (Also see 5.D.1. guideline above)	New façades along public streets and public spaces: <ul style="list-style-type: none">•Buildings < 4,000 sq. ft.: 15 ft.•Buildings ≥ 4,000 sq.ft.: 18 ft. for at least 50% of the façade, unless located along a primary street– see 5.D.3 below. Ground floors of multistory non-residential buildings: min. 10 ft., floor to ceiling.	
Maximum heights	T4 and T4-N	3 stories
	T4-I	6 stories
	T5 or T5N	5 stories
	T5-I	10 stories
Maximum heights with a) mix of use, b) public space, and/or c) structured parking bonuses (See Section 5.D.3.)	T4-N: a, b, <u>or</u> c	4 stories
	T4: a, b, <u>or</u> c	5 stories
	T4: <u>a&b</u> , <u>a&c</u> , or <u>b&c</u>	6 stories
	T5 or T5N: a, b, <u>or</u> c	8 stories
	T5 or T5N: <u>a&b</u> , <u>a&c</u> , or <u>b&c</u>	10 stories

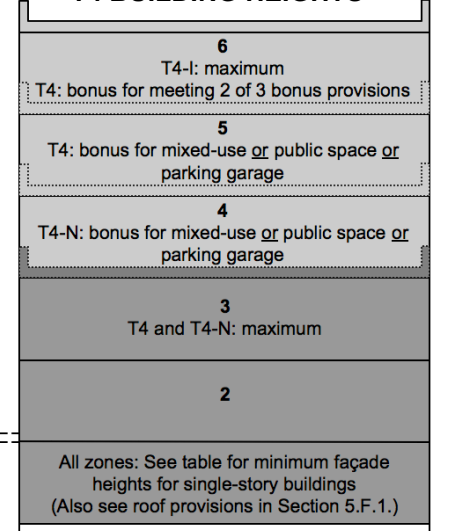
3. BUILDING HEIGHT ALONG PRIMARY STREET

Two (2) stories consisting of a minimum of 18 feet along the following streets:

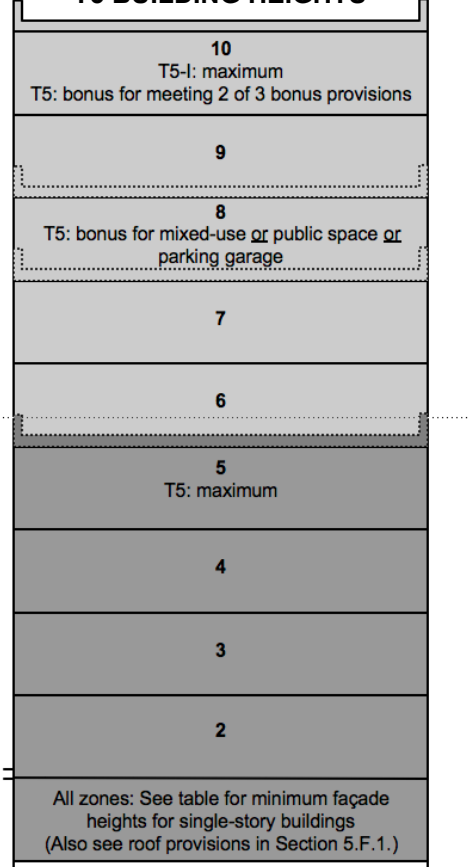
- W. Vickery Boulevard from Adams Street to S. Main Street
- Pennsylvania Avenue from 8th Avenue to S. Main Street
- W. Rosedale Street from Jerome Street to Evans Avenue
- W. Magnolia Avenue from 8th Avenue to S. Main Street
- Park Place Avenue from FWRR to 8th Avenue
- 8th Avenue from Pennsylvania Avenue to Park Place Avenue
- S. Henderson Street from I-30 to W. Magnolia Avenue
- S. Hemphill Street from W. Vickery Boulevard to W. Allen Avenue
- S. Jennings Avenue from W. Vickery Boulevard to W. Magnolia Avenue
- S. Main Street from W. Vickery Boulevard to W. Magnolia Avenue

Refer to map in Section 3.F.

T4 BUILDING HEIGHTS



T5 BUILDING HEIGHTS

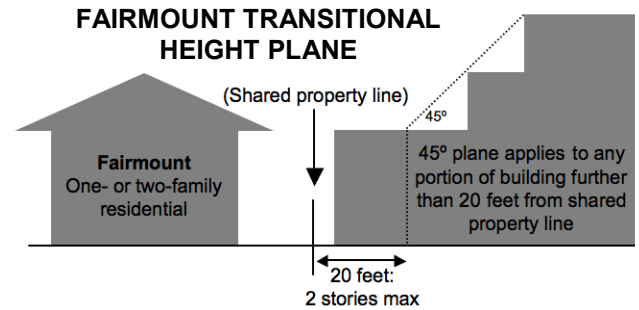


5.D. Building Height

3. HEIGHT BONUS REQUIREMENTS

- a. **Mix of residential and non-residential uses** – The project must include at least 20% residential and 10% office, restaurant, and/or retail uses, as measured by gross floor area. The UDC is authorized to reduce the required percentages by up to 50% for exceptional projects.
- b. **Public space** – The project must include a publicly accessible, privately maintained park, plaza, or other usable outdoor public space. Public access must be maintained through a public access easement. The ratio of the project's gross floor area (including height bonus area) to the public space's surface area shall not exceed 10:1, and no public space shall be less than 2,500 sq. ft. (See Sec. 5.B.4.)
- c. **Structured parking** – Project must provide at least 75% of off-street spaces within a multilevel garage. The number of garage levels are not limited except that the garage height shall not exceed the tallest non-garage building. Also see 5.E.7.

4. **FAIRMOUNT TRANSITIONAL HEIGHT PLANE** – Properties that share a property line with a one- or two-family house in the Fairmount Historic District: Any portion of a building within 20 ft. of the property line shall not exceed 2 stories. A 45° transitional height plane shall apply to any portion of a building further than 20 ft. from the property line, as depicted to the right.



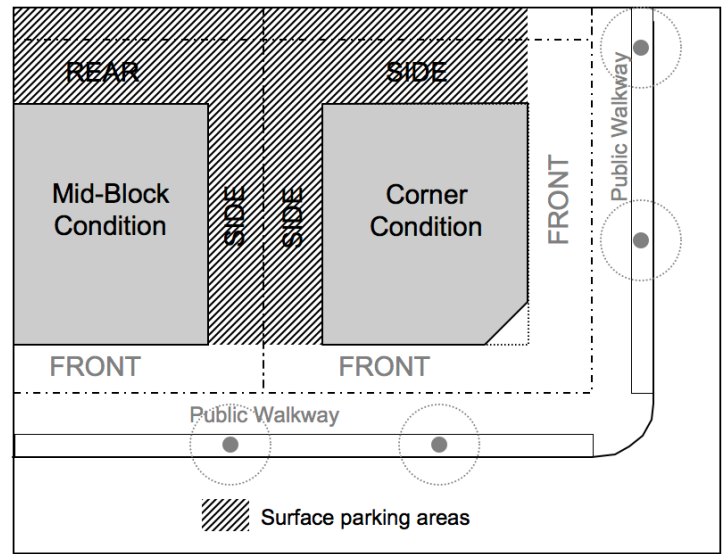
5.E. Parking and Driveways

1. **OFF-STREET PARKING REQUIREMENT** – None, except for properties located within 250 feet of a one- or two-family zoning district. For those properties, the requirements in Section 6.201 of the Zoning Ordinance, reduced by 25%, shall apply. Uses within historically significant buildings are exempt.
GUIDELINE – Shared parking garages are encouraged.
2. **SURFACE PARKING CAP** – The number of off-street spaces shall not exceed 100% of the total prescribed by Section 6.201 of the Zoning Ordinance, unless a parking study demonstrates need for additional spaces.
3. **SURFACE PARKING LOTS** – Lots shall be located behind or to the side of buildings as depicted in the diagrams to the right. (Parking lot design standards are contained in Section 6.201 of the Zoning Ordinance.)
GUIDELINE – Surface parking lots that front streets are discouraged; all site plan options that minimize surface lots along public streets should be explored.
4. **MAXIMUM SURFACE PARKING LOT FRONTAGE**– The percentage of parking lot frontage along any street shall not exceed the following maximums:
 - a. 40% of the development site's total frontage length along a project's primary streets, and
 - b. 70% of the development site's total frontage length along a project's secondary streets.

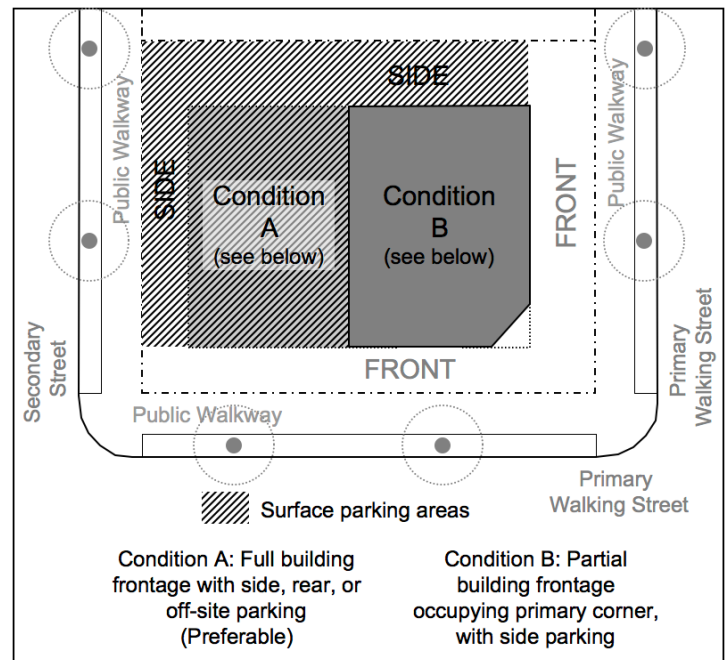
Parking lot frontage measurements shall include the combined frontage length of any paved and/or drivable surface that functions as part of a parking lot's circulation, such as – but not limited to – drive aisles and parking spaces.

5. **SURFACE PARKING SCREENING** – Parking lots that front a street shall be separated from the sidewalk by a decorative and durable screen at least 4 ft. in height.
GUIDELINE – Screening walls attached to buildings should be designed as architectural extensions of the building, constructed of the same materials and style.
6. **RESIDENTIAL GARAGES** – Garages should not front public pedestrian space. Garages shall be accessed from alleys or rear driveways or shall be detached and located at the rear of the site.
7. **COMMERCIAL OR PUBLIC PARKING GARAGES** shall be located at the interior of a block or underground and shall not be visible from public rights-of-way, unless the UDC approves an exception. Ground floor commercial uses, or space adaptable for future commercial use, should be integrated into parking garages located along public streets.
8. **DRIVEWAY LOCATION** – Private vehicular driveways shall not be located along "Main" streets. This provision applies as a guideline in "I" zones.
9. **DROP-OFF AND LOADING AREAS** – On-street drop-off and loading areas support a pedestrian-oriented district and may be approved by the City's Traffic Engineer. Off-street loading and drop-off areas that are located between the building and the sidewalk are allowed in T4-I and T5-I zones if a continuous sidewalk is provided adjacent to the street.
GUIDELINE – Drop-off and loading areas proposed in zones other than T4-I and T5-I should be located under the overhang of a building that maintains the urban street edge within the maximum front setback.

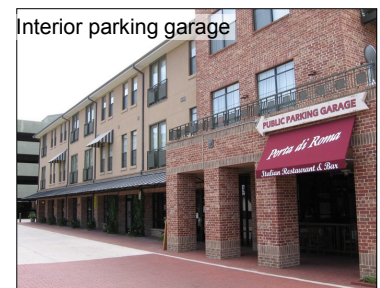
SURFACE PARKING: PARTIAL BLOCKFACE CONDITIONS



SURFACE PARKING: FULL BLOCKFACE CONDITIONS



COMPATIBLE PARKING FACILITIES



The parking standards are intended to minimize the impact of parking facilities.

5.F. Architectural Standards

1. ROOFS

- a. **Single-story buildings** – New single-story non-residential buildings shall not have large expanses of sloped roofs. Roof slope shall not be greater than 1:12. Sloped roof elements that are not part of the actual roof, similar to elements found on certain historic commercial buildings, are permitted.
- b. **Roof design guidelines** –
 - Parapet roofs or low-sloped roofs are encouraged.
 - Visually dominant pitched roofs on multi-story buildings are strongly discouraged, but partial pitched roofs or pitched roof elements are generally compatible.
 - “Green” roofs that utilize plants to absorb rainwater and reduce ambient air temperatures are strongly encouraged.
- c. **Screening of rooftop equipment** –Parapets or other screening elements of sufficient height shall conceal mechanical equipment from street-level views.

2. **PARALLEL FRONTAGES (Guideline)** – Building facades should be built parallel to the street frontage, except for chamfered corners.
3. **ENTRANCES** facing a pedestrian way shall incorporate elements that protect pedestrians from the sun and rain.
4. **AWNINGS, GALLERIES, ARCADES, AND BALCONIES (Guidelines)** – Awnings, galleries, and arcades are encouraged for all ground floor retail uses so as to provide pedestrian protection and visual interest. Where feasible, balconies are encouraged and should be designed as an integral part of the building
5. **FAÇADES** – **Facades facing public streets** and other public spaces (except alleys). For these facades, all standards in this section apply. **Other highly visible facades** that meet any of the following conditions must also comply with the standards in this section, with the exception of 5.F.5.d. “Ground floor transparency,” which shall not apply.

- **Parking lot facing facades:** Facades visible from a public street or other public space and that face a surface parking lot of the same development project.
- **Side facades:** Facades visible from a public street or other public space as a result of sharing a side property line with an adjacent building that is set back at least 15 feet further than the proposed building.
- **Railroad facades:** Facades visible from public streets and other nearby public areas as a result of facing a railroad right-of-way.

ROOFS ON SINGLE-STORY BUILDINGS



PEDESTRIAN PROTECTION



ENTRANCES FROM PEDESTRIAN WAYS



FAÇADE VARIATION



5.F. Architectural Standards (cont.)

- a. **Façade variation** – Buildings shall, at a minimum, incorporate:
 - 1) Expression of structural elements, such as:
 - o Floors
 - o Vertical support
 - o Foundation
 - 2) Façade articulation through the use of projecting and recessed elements.
 - 3) Variety in materials, material pattern, or color.
- b. **Building materials**
 - 1) New building façades facing public areas (except alleys) shall conform to the material standards listed to the right. The list could evolve as technology changes. Creative design is encouraged, and projects that incorporate unlisted materials may be submitted for staff and UDC review.
 - 2) Leadership in Energy and Environmental Design (LEED) Buildings – Buildings following the U.S. Green Building Council's LEED certification requirements shall be exempt from building material standards.
- c. **Fenestration (all buildings)** – New building façades fronting on publicly accessible streets or other public spaces (except alleys) shall have openings and transparent (not mirrored) glazing that together constitute not less than 25 percent of the façade. This provision applies as a guideline in “I” zones.
- d. **Ground floor transparency for non-residential uses**
 - 1) For all ground floor nonresidential uses located along publicly accessible streets and other public spaces, at least 40 percent of the wall area between 2 and 12 feet shall consist of doors and windows and transparent (not mirrored) glazing. Clear glazing must have a visible transmittance rating of 0.5 or greater to count towards the fenestration requirement. This provision applies as a guideline in “I” zones.
 - 2) Guideline – Ground floor restaurants, cafés, and bars are encouraged along “Main” streets and should have windows and doors that can be opened to provide direct access to the sidewalk when weather permits (i.e., when interior heating or cooling systems are not necessary.)
- e. **Lower level delineation for nonresidential or mixed-use buildings** – Façades oriented to a publicly accessible street or other public space shall include clear delineation between the first or second level and the upper levels with a cornice, canopy, balcony, arcade, or other architectural feature.
- f. **Parking garage façades on “Main” Streets** – Parking garages shall not have exposed structured parking at the ground floor level along a “Main” street.

MATERIALS ALONG PUBLIC STREETS



BUILDING MATERIALS

Primary Materials

- Brick
- Stone, or stone veneer with cavity wall construction
- Stucco
- Glass curtain wall system
- Metal Panels – individual or curtain wall systems
- Concrete – finish should be to an architectural level
- Wood
- Cement composite board (not simulating wood)
- Tile – terra cotta, porcelain, or ceramic

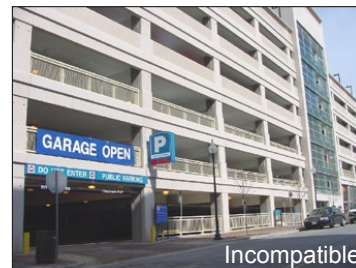
Limited Accent and Trim Materials

- All primary materials listed above
- Metal – galvanized, painted, or ornamental
- Pre-cast masonry (trim and cornice only)
- EIFS (Exterior insulation and finish system)
- Concrete fiber simulated wood siding

Inappropriate Materials

- Applied stone without cavity wall construction
- Vinyl or aluminum siding
- Mirrored glass

GARAGE FAÇADES ON “MAIN” STREETS



5.F. Architectural Standards (cont.)

6. ARCHITECTURAL VARIETY IN LARGE PROJECTS – Each sequential block of new construction shall contain unique building façades so as to encourage architectural variety within large projects. While the use of similar architectural elements on multiple buildings is acceptable; large, visually monotonous projects that repeat the same exterior buildings design(s) and do not contribute to visual diversity and human-scale character are inappropriate.
7. OUTDOOR STORAGE OR DISPLAY
 - a. **T4, T4-N, and T5 zones** – Development in these zones shall comply with the standards for outdoor storage and display set forth in Section 5.306 of the Zoning Ordinance. The UDC may consider exceptions if outdoor materials are sufficiently screened by decorative fencing or other screening elements.
 - b. **“I” zones** – Decorative fencing or other screening elements shall be used to minimize the visual impact of outdoor storage adjacent to public streets or other public spaces.
8. MOBILE FOOD COURT REVIEW– The requirements of the mobile vending food court ordinance apply. Because of the diverse development conditions and unique mix of residential and non-residential uses within the Near Southside, all proposed mobile vending food courts within NS districts shall be reviewed by the Urban Design Commission for contextual compatibility with neighboring structures.

Outside of driveways, bathrooms and other facilities required by the base ordinance, all areas shall be landscaped, shaded, or otherwise improved to create spaces that are comfortable and appealing for park users during business hours, and are also visually attractive and complimentary to the court's surroundings during off hours. Bathroom buildings are exempt from transparency and fenestration requirements. All building material standards shall apply.

9. SIGNS –

a. Types

- 1) On premise attached signs are allowed, with the exception of internally-lit cabinet signs and flat signs that have a projecting depth of less than six inches and monument style signs are permitted.
- 2) Monument signs are discouraged and shall require Urban Design Commission review. UDC-approved monument signs should be limited to more suburban-style site layouts that have a significant front-yard setback, or other unique circumstances.
- 2) Pole signs are not permitted.
- 3) Off premise signs are not permitted.

b. **Basic Sign Standards** – Signs shall conform to the basic dimensional regulations set forth in Chapter 6, Article 4 of the Zoning Ordinance with the following exceptions for projecting signs:

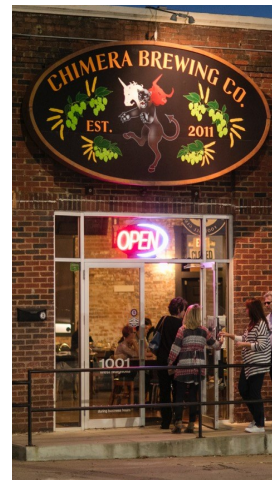
Maximum projection: Four (4) feet

Exception: When existing signs on the same block face project further than 4 feet, the new sign may match the smallest projection over 4 feet.

c. **Design Guidelines** – Signs should conform to the design guidelines contained in Appendix A, excerpted from the Downtown Sign Standards. The following are general guidelines for sign design:

- 1) Location: Signs should be positioned to emphasize or accent building elements such as storefront openings or entrances.
- 2) Alignment: Signs should be aligned with those on neighboring buildings to promote visual order on the block, to avoid visual clutter, and to enhance legibility.
- 3) Dimensions and Scale: Signs should be consistent with the human scale of Near Southside buildings and blocks.
- 4) Number of Signs: The number of signs per building façade should be limited to the fewest necessary to clearly identify businesses located within the building.
- 5) Colors and Materials: Signs should relate in color and material with the building facade and streetscape. Colors should contribute to legibility and visual appeal.
- 6) Graphics: Sign graphics should be simple. Symbols and logos on signs are quickly read and easily remembered. Lettering should be in proportion to the size of the sign.
- 7) Lighting: Indirect illumination is recommended. Lighting sources should be external, shielded, and directed only at the sign. Internal illumination is acceptable when the letters themselves – not the background – are lit.

SIGNS



The signs above are all consistent with the design guidelines contained in Appendix A.

5.G. On-Site Landscaping (Also see Section 5.B.3.)

1. TREE PRESERVATION AND CANOPY REQUIREMENTS – The following provisions of the Tree Ordinance apply:
 - a. **Preservation Of Significant Trees** – Significant trees at least 30 inches in diameter (94.25 inches in circumference) can only be removed by permit of the City Forester. Criteria for the permit will be one of the following conditions:
 - 1) Retention of other existing trees on the same site with a canopy area 1 ½ times the area of the specific tree's canopy, or
 - 2) Removal will be granted if planting of new trees at five (5) times greater in canopy area than the removed specific tree canopy (refer to list of canopy trees on the following page), or
 - 3) Payment into the tree fund based upon the total diameter of the specific tree times \$200 per diameter inch, or
 - 4) Urban Forestry Board approved plan that mitigates the removal of the large tree.Replacement of any tree preserved and that dies within five years due to construction or development activities will be the responsibility of the original applicant. Replacement will be new trees with a minimum of 3 inches each in diameter and equal to five times the lost canopy. Tree replacement will be guaranteed for a period of two years.
 - b. **Trees In Surface Parking Lots** – All new construction projects shall provide at least 40% tree canopy coverage of surface parking lots (refer to the categorized list of canopy trees on the following page).
 - c. **Tree Protections and Warranty/Replacement** – The provisions of Zoning Ordinance Chapter 6, Article 3, Section 6.301.K.6. apply. These provisions ensure that sufficient care is taken during construction to protect existing trees, and that any preserved or newly planted tree required by the provisions of Sections 5.G.1. and 5.B.3. In this document shall be replaced if it dies within 5 years.
2. FRONT YARD LANDSCAPING GUIDELINE – Although no front yard setback is required, where there is a setback, front yard landscaping in addition to required trees should be provided and maintained in areas outside of ground level elements such as patios or porches. Native and drought tolerant species should be used, and irrigation systems should be designed to minimize water demand.

CANOPY TREES APPROVED BY THE CITY FORESTER (Other species considered on case-by-case basis.)

Large Canopy Trees (2,000 square feet)

Pecan #	Carya illinoensis
Deodar Cedar	Cedrus deodara
Green Ash	Fraxinus pennsylvanica
Southern Magnolia#	Magnolia grandiflora
Bur Oak*	Quercus macrocarpa
Chinquapin Oak	Quercus muhlenbergii
Shumard Oak #	Quercus shumardii
Texas Red Oak	Quercus buckleyi
Live Oak *	Quercus virginiana
American Elm	Ulmus Americana
Cedar Elm *	Ulmus crassifolia
Lacebark Elm	Ulmus parvifolia

Medium Canopy Trees (700 square feet)

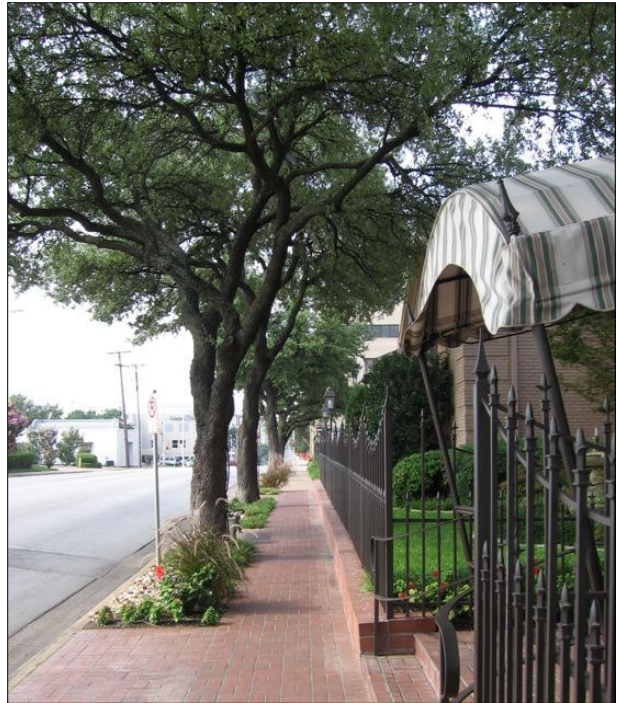
Caddo Maple *	Acer barbatum var. Caddo
Bigtooth Maple *	Acer grandidentatum
Common Persimmon	Diospyros virginiana
Texas Ash	Fraxinus texensis
Ginkgo	Ginkgo biloba
Kentucky Coffeetree	Gymnocladus dioica
Eastern Red-Cedar *	Juniperus virginiana
Eldarica (Afghan) Pine *	Pinus eldarica
Italian Stone Pine	Pinus pinea
Honey Mesquite *	Prosopis glandulosa
Blackjack Oak *	Quercus marilandica
Monterrey (Mex white) Oak *	Quercus polymorpha
Pond Cypress	Taxodium ascendens
Bald Cypress *	Taxodium distichum

Small Canopy Trees (100 square feet)

Japanese Maple #	Acer palmatum
Common button-bush	Cephalanthus occidentalis
Redbud *	Cercis Canadensis
Desert Willow *	Chilopsis linearis
Rough-leaf Dogwood #	Cornus drummondii
Texas Persimmon *	Diospyros texana
Carolina buckthorn #	Frangula caroliniana
Yaupon Holly *	Ilex vomitoria
Deciduous Holly	Ilex deciduas
Crepe Myrtle *	Lagerstroemia incisa
Mexican Plum *	Prunus mexicana
White Sin Oak *	Quercus sinuata var. breviloba
Flameleaf Sumac *	Rhus lanceolata
Eve's Necklace *	Sophora affinis
Mexican buckeye *	Ugnadia speciosa
Rusty Blackhaw	Viburnum rufidulum

* Drought tolerant species

Not recommended for parking lots or high heat areas



Tree preservation standards promote the retention of significant trees like these along 8th Avenue.



Trees planted in parking lots, together with pervious surfaces such as permeable concrete, improve environmental quality and help reduce stormwater runoff.

6.A. Mix of Use Requirement for Large Developments

The following standards are intended to ensure that large projects in mixed-use zoning districts include a mix of uses, or contribute to the creation of a larger mixed-use area. These standards do not apply in "I" zones.

1. **CONCEPTUAL LAND USE PLAN** – Developers of projects equal to or larger than 3 acres in size shall submit a conceptual land use plan for approval by the Planning Director. The conceptual land use plan must be approved before a building permit application is accepted. The Planning Director may require a conceptual land use plan for a project smaller than 3 acres if it is part of a project larger than 3 acres. The conceptual land use plan shall illustrate the location and calculated land area of land uses on the site, using the following land use categories (see Section 6.C. for detailed list of land uses within these categories; exceptions noted below):

- One- or two-family residential
 - Multifamily residential
 - Industrial and commercial (including medical offices)
 - Public and civic uses (excluding parks and medical offices)
 - Mixed-use buildings (must include at least 20% residential and 10% non-residential)
 - Publicly accessible parks and plazas
- Parking facilities and private open spaces shall be classified the same as the primary land use they serve.

2. **PROJECT TEST** – The conceptual land use plan shall be approved if it shows that:
 - a. The project includes uses within at least two of the land use categories, and
 - b. No land use category other than mixed-use buildings occupies greater than 2/3 of the total land area.
 If a project does not comply with the project test, then the vicinity test in Section 3. shall apply.

3. **VICINITY TEST** – Developments not complying with the project test are permitted if:
 - a. The Planning and Development Director determines that the following conditions are satisfied:
 - 1) The proposed land use at any location within the proposed development site must be within a walking distance of 1,000 feet of a different land use, as measured by the shortest pedestrian route, and
 - 2) The percentage of any single land use category other than mixed-use buildings within a 1,000-foot radius of any location within the proposed development site shall not be greater than 2/3 of the total land area within the radius. The proposed development shall be included in the calculation of this percentage. Undeveloped or agricultural property located within the radius shall not be included in the calculation; or
 - b. The Director determines that the developer has demonstrated that unique site conditions (e.g. adjacency to natural features, highways, freight yards, etc.) make compliance with the conditions of section a. above impractical in certain areas of the development site.

MIX OF USE TESTS



Projects of at least 3 acres must provide a mix of uses that conforms to the "project test" requirements of Section 2. or must contribute to the creation of a larger mixed-use area. City staff conducts a "vicinity test" to make this determination for large single-use projects.

6.B. Special Requirements in “N” Neighborhood and “R” Restricted Zones

The following standards are intended to enhance the residential character of existing neighborhoods and to promote the development of additional housing in those areas.

1. RESIDENTIAL REQUIREMENT IN “N” ZONES – Single-use buildings of commercial, retail, or other non-residential character are prohibited in “N” neighborhood zones. (See development zone maps in Section 4.B.). Buildings within these zones must include residential uses that constitute at least 40% of gross floor area within each two-story building, and at least 60% within each building with more than two stories. Outdoor spaces for residential use may be included in the percentage. For this section only, bed and breakfast uses are considered residential. Buildings that incorporate a mix of residential and non-residential uses are allowed and encouraged (see height bonus for mix of uses in Section 5.C.)



Oleander Plaza

2. PROHIBITED USES IN “R” ZONES – Certain uses, including bars and light industrial uses, are prohibited in “R”

3. PROHIBITED USES IN “N” ZONES – Surface parking lots providing parking for single use non-residential projects.

6.C. Permitted Land Uses

The following table lists land uses permitted in the NS District. A (P) indicates that these uses are allowed by right in the applicable development zone. A (SE) indicates that these uses are allowed by special exception from the Board of Adjustment. An asterisk (*) indicates that the uses are allowed but are subject to supplemental development standards. Please refer to the the Zoning Ordinance for all use definitions and any referenced supplemental standards.

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
RESIDENTIAL USES					
Household Living	One-family detached dwelling	P	P	P	
	One-family-attached (townhouse, rowhouse)	P	P	P	
	Multifamily dwelling (apartment)	P	P	P	
	HUD-code manufactured housing				
	Industrialized housing	P*	P*	P*	
	One dwelling unit when part of a business	P	P	P	
Group Living	Boarding or lodging house				
	Community home	P*	P*	P*	5.115
	Fraternity or sorority house	P	P	P	
	Group home I	P*	P*	P*	5.115
	Group home II	P*	P*	P*	5.115
	Halfway house				4.305B
	Shelter				
PUBLIC AND CIVIC USES					
Education	College or university	P	P	P	
	Day care center (child or adult)	P	P	P	
	Kindergarten	P	P	P	
	School, elementary or secondary (public or private)	P	P	P	

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
PUBLIC AND CIVIC USES (cont.)					
Government (cont.)	Animal shelter				
	Correctional facility				
	Government maintenance facility				
	Government office facility	P	P	P	
	Museum, library or fine art center	P	P	P	
	Probation or parole office			P*	5.127
Health Care Facilities	Ambulance dispatch station	P	P	P	
	Assisted living facility	P	P	P	
	Blood bank	P	P	P	
	Care facility	P	P	P	
	Health services facility; including doctor's office or medical clinic	P	P	P	
	Hospice	P	P	P	
	Hospital	P	P	P	
	Massage Therapy and Spa	P	P	P	
	Nursing home (with full medical services)	P	P	P	
Recreation	Center, community recreation or welfare	P	P	P	
	Center, community recreation or welfare, private or non-profit	P	P	P	
	Country club (private)	P	P	P	
	Country club (public)				
	Golf course	P	P	P	
	Golf driving range			P	
	Neighborhood recreation center	P	P	P	
	Park or playground (public or private)	P	P	P	
Religious	Place of worship	P	P	P	
	Place of worship auxiliary use	P	P	P	
Utilities	Electric power substation	SE	SE	P	
	Power plant or central station light				
	Stealth telecommunications towers	P*	P*	P*	5.137
	Telecommunications antenna (on structure)	P*	P*	P*	5.136
	Telecommunications tower	SE*	SE*	SE*	5.137

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
COMMERCIAL USES					
Utilities (cont.)	Utility transmission or distribution line, Natural gas compressor stations	P*	P*	P*	5.14
	Wastewater (sewage) treatment facility				
	Water supply, treatment or storage facility	SE	SE	SE	
Entertainment and Eating	Amusement, outdoor				
	Baseball/softball facility (commercial)	P	P	P	
	Bowling alley	P	P	P	
	Bar, tavern, cocktail lounge; club, private or teen		P	P	
	Circus				
	Club, commercial or business	P	P	P	
	Drive-in restaurant or business	P	P	P	
	Gambling facility (including bingo)				
	Health or recreation club	P	P	P	
	Indoor recreation	P	P	P	
	Lodge or civic club	P	P	P	
	Massage parlor				
	Museum/cultural facility	P	P	P	
	Racing; horse, dog or automotive				
	Restaurant, café or cafeteria	P	P	P	Chap 9, Rest.
	Sexually oriented business				
	Shooting or weapons firing range				
	Stable, commercial, riding, boarding or rodeo arena				
	Swimming pool, commercial	P	P	P	
	Theater, drive-in				
	Theater, movie theater or auditorium	P	P	P	
Lodging	Bed and breakfast inn	P	P	P	
	Bed and breakfast home	P	P	P	
	Hotel, motel or inn			P	
	Recreational vehicle (RV) park				
Office	Bank, financial institution	P	P	P	
	Offices	P	P	P	

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
COMMERCIAL USES (cont.)					
Retail Sales and Service	Antique shop	P	P	P	
	Appliance, sales, supply or repair	P*	P*	P*	
	Bakery	P	P	P	
	Barber or beauty shop	P	P	P	
	Boat rental or sales			P	
	Book, stationery stores or newsstand	P	P	P	
	Burglar alarms sales or service	P	P	P	
	Business college or commercial school	P	P	P	
	Caterer or wedding service	P	P	P	
	Clothing/wearing apparel sales, new	P*	P*	P*	
	Clothing/wearing apparel sales, used	P*	P*	P*	
	Convenience store	P	P	P	
	Copy store or commercial print center without off-set printing	P	P	P	
	Dance studio	P	P	P	
	Dressmaking, custom; millinery shop	P	P	P	
	Duplicating services	P	P	P	
	Farmer's Market	P	P	P	
	Feed store, no process/milling	P	P	P	
	Firewood sales	P*	P	P*	5.113
	Furniture sales, new and used (office & residential) in a building	P*	P*	P*	5.134
	Furniture upholstery, refinishing or resale	P	P	P	
	General merchandise store	P	P	P	
	Greenhouse or plant nursery	P	P	P	5.114
	Grocery store, meat market	P	P	P	
	Gunsmithing, repairs or sales			P	
	Home improvement store	P	P	P	
	Interior decorating	P	P	P	
	Kennel				
	Large retail store	P	P	P	
	Laundry or dry cleaning collection office	P	P	P	
	Laundry, dry cleaning or washeteria	P	P	P	
	Leather goods shop	P	P	P	

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
COMMERCIAL USES (cont.)					
Retail Sales and Service (cont.)	Liquor or package store	P	P	P	
	Locksmith	P	P	P	
	Medical supplies/equipment sales or rental	P	P	P	
	Mini-warehouses				
	Mobile food court	P*	P*	P*	5.406
	Mortuary or funeral home	P	P	P	
	Newspaper distribution center			P	
	Optician	P	P	P	
	Pawn shop			P*	5.125,
	Pharmacy (drug store)	P	P	P	
	Photograph, portrait/camera shop or photo finishing	P	P	P	
	Recording studio	P	P	P	
	Retail sales, general	P	P	P	
	Saddle or harness, repair or sales	P	P	P	
	Shoe shine shop	P	P	P	
	Studio, art or photography	P	P	P	
	Tailor, clothing or apparel shop	P	P	P	
	Tattoo parlor			P	
	Taxidermist shop	P	P	P	
	Veterinary clinic with indoor kennels	P*	P*	P*	5.142
	Veterinary clinic with outdoor kennels				
Vehicle Sales and Service	Auto parts supply, retail	P	P	P	
	Automotive repair; paint and body shop	P	P	P	5.104
	Car wash, full or self service			P*	5.108
	Gasoline sales	P	P	P	
	Mobile home or manufactured housing sales				
	Parking area or garage, commercial or auxiliary	P*	P*	P*	6.202F
	Recreational vehicle (RV) sales/service			P	
	Service station			P*	5.104
	Truck stop w/ fuel and accessory services				
	Vehicle junkyard				
	Vehicle sales or rental; including automobiles, motorcycles, boats or trailers	P	P	P	
Vehicle steam cleaning				P	

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES					
Light Industrial Services	Assaying			P	
	Assembly of pre-manufactured parts, except for vehicles, trailers, airplanes or mobile homes		P	P	
	Blacksmithing or wagon shop				
	Bottling works, milk or soft drinks			P	
	Carpet and rug cleaning			P	
	Chicken battery or brooder				
	Coal, coke or wood yard				
	Crematorium			P	
	Electroplating			P	
	Fabricating or manufactured housing; temporary or office building				
	Food processing (no slaughtering)			P	
	Furniture or cabinet repair or construction		P	P	
	Furniture sales w/ outside storage/display (new/used)				
	Galvanizing, small utensils				
	Machine shops			P	
	Manufacture of artificial flowers, ornaments, awnings, tents, bags, cleaning/polishing preparations, boats under 28 ft in length, brooms or brushes, buttons & novelties, canvas products, clothing, suits, coats, or dresses for wholesale trade, plastics		P	P	
	Manufacture of aluminum, brass or other metals or from bone, paper, rubber, leather		P	P	
	Manufactured home/RV repair				
	Monument/marble works, finishing and carving only			P	
	Monument works, stone			P	
	Outdoor sales and storage		P*	P*	5.133
	Paint mixing or spraying			P	
	Paper box manufacture			P	
	Pattern shop			P	
	Printing, lithographing, book-binding, newspapers or publishing			P	

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES (cont.)					
Light Industrial Services (cont.)	Rubber stamping, shearing/punching			P	
	Rubber stamp manufacture			P	
	Sheet metal shop			P*	5.131
	Warehouse or bulk storage			P	
	Welding shop, custom work (not structural)		P	P	
	Yards, contractor's, lumber or storage, automobiles, storage yards, building material			P*	5.133
Heavy Industrial and Manufacturing	Animal by-products processing				
	Batch plant, concrete or asphalt (permanent)				
	Brewery, distillery or winery			P	
	Brick, clay, glass, shale, tile or terra cotta products manufacture				
	Cement products plant				
	Cement, lime, gypsum or plaster of Paris manufacture				
	Cotton gin, cotton oil mill, bailing or compress				
	Creosote, treatment/manufacture				
	Egg cracking or processing				
	Furnace, blast; forge plant, boiler works manufacture				
	Galvanizing, sheet or structural shapes				
	Gas (natural or artificial) manufacture processing/storage				
	Glue manufacture				
	Grain elevator				
	Magnesium casting, machining or fabricating				
	Manufacture of acetylene or oxygen gas, alcohol, computers and related electronic products, airplanes, automobiles, trucks & tractors (including assembly plants), ball or roller bearings, steel tanks, candles & celluloid, cash registers, cutlery, disinfectants, dextrin, dyestuff, electrical machinery, farm tools, typewriters and vinegar				
	Manufacture of basket material, bicycles, boots, boxes, other than paper, caskets, shoes				

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES (cont.)					
Heavy Industrial and Manufacturing (cont.)	Manufacture of dies, cores, die-casting molds				
	Manufacture, processing/production of hazardous chemicals				
	Metal casting				
	Metal foundry plant or fabrication plant				
	Metal smelting, reclamation or ore reduction				
	Metal stamping, dyeing, shearing or punching				
	Mill, feed or flour				
	Mining quarry, dredging or excavation of rock, dirt, gravel, sand, stone				
	Packing plant				
	Paper or pulp manufacture				
	Petroleum refining or wholesale storage				
	Planing mill or woodworking shop				
	Poultry killing or dressing				
	Rock, cement crushers & stone quarry				
	Rolling mill				
	Soap manufacture				
	Soda or compound manufacture				
	Stoneyard, building stone, cutting, sawing or storage				
	Tar distillation/manufacturing				
	Tobacco (chewing) manufacture or treatment				
	Welding shop				
Transportation	Airport, aviation field, helistop or landing area	SE	SE	SE	
	Passenger station	P	P	P	
	Railroad freight or classification yard				
	Railroad roundhouse or RR car repair shop				
	Railroad tracks: team, spur, loading or storage				
	Terminal; truck, freight, rail or water				
Waste Related	Landfill, recycling center, household hazardous waste or waste tire facility				
	Pet cemetery				

6.C. Permitted Land Uses (cont.)

		NS-T4R	NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES (cont.)					
Waste Related (cont.)	Recycling collection facility	SE*	SE*	P*	5.13
	Salvage yard (other than automotive)				
Wholesale Trade	Wholesale: bakery, produce market or wholesale house			P	
	Wholesale office or sample room		P	P	
OTHER USES					
Agriculture	Agricultural				
	Stockyards or feeding pens (commercial)				
ACCESSORY USES					
Accessory Uses	Accessory use or building	P	P	P	
	Home occupation	P	P	P	
	Recreation area (private), indoor or outdoor	P	P	P	
	Satellite antenna (dish)	P*	P	P*	5.304
	Stable, stockyards or feeding pens (noncommercial)				
	Storage or display, outside	P*	P*	P*	5.306
TEMPORARY USES					
Temporary Uses	Amusement, outdoor (temporary)	SE*	SE*	SE*	5.4
	Batch plant, concrete or asphalt (temporary)				5.401
	Garage or other occasional sale	P*	P*	P*	5.402
	Model home	P*	P*	P*	5.403
	Residence for security purposes, temporary	SE*	SE*	SE*	5.404
	Trailer, portable; sales, construction or storage	P	P	P	
	Vendor, door-to-door	P*	P*	P*	9.101
	Vendor, Food, Non-Potentially Hazardous Food	P*	P*	P*	5.406
	Vendor, Food, Potentially Hazardous Food	P*	P*	P*	5.406
	Vendor, Merchandise	P*	P*	P*	5.406
	Vendor, Transient	P*	P*	P*	9.101
	Vendor, Transient, Non-Potentially Hazardous Food	P*	P*	P*	5.406
	Vendor, Transient, Potentially Hazardous Food	P*	P*	P*	5.406